



January 8, 2015

Ref: 29011.00

Mr. Michael Sassi, P.E.
Regional Highway Work Permit Coordinator
New York State Department of Transportation
4 Burnett Boulevard
Poughkeepsie, NY 12603

Re: MDP and Site Plan Phase 1 Comments
Silo Ridge Resort Community
Amenia, NY

Dear Mr. Sassi:

On behalf of our client, Silo Ridge Ventures, LLC., "the Applicant", VHB Engineering, Surveying and Landscape Architecture, P.C. (VHB) offers the following responses to your comments dated October 17, 2014 (numbered in accordance with your letter).

Conceptual Comments

1. Comment: The project proposes a main entrance on RTE 22 employing a dual egress alignment. The Applicant has submitted Synchro (traffic modeling software) files showing the projected level of service overall and for individual turning movements. Upon review, it is found that the operation of the proposed intersection is acceptable for all turning movements with the exception of the eastbound left turn out (northbound) from the development. This turning movement is shown in the model to operate at a level service, "F", which is generally found to be an unacceptable level of service because of the long delay. From a state highway perspective, the concern is that motorists may become impatient turning left when it not safe to do so. As such, the Applicant is asked to consider alternate means of northbound egress, propose a restriction of this movement, or otherwise justify the movement from an operational and safety perspective.

Response: Subsequent to VHB's August 12, 2014 submission to NYSDOT, the phasing of the project has been modified, with more development occurring in Phase 1. VHB has updated the trip generations, volume projections and analyses based on the change in development and provides justification for the left-turn exiting movement, as described in the following.



The Silo Ridge development's residential component will be exclusively for second-home ownership. As such, VHB has reviewed available trip generation data to determine the appropriate residential trip rates to use in the analysis. Research data (attached) indicates that second-home residences generate between 26 and 38 percent of the trip rates for single-family homes contained in the *ITE Trip Generation Manual, Ninth Edition*. A review of the range of peak hour trip rates for ITE Land Use Code 210 ("Single-Family Detached Housing") indicates that the lowest surveyed rates constitute less than 48 percent of the average ITE trip rate. Based on this information, it is realistically anticipated that the Silo Ridge residential homes will only generate half as much traffic as projected by ITE for primary residences and as evaluated in the previous study dated March 12, 2014. The previous analysis was revised to reflect the lower anticipated residential trip generation rates. The revised trip generations for the Silo Ridge Modified Project are indicated in Table 1.

Table 1 – Peak Hour Trip Generation

Development	Size	AM Peak Hour			PM Peak Hour			Saturday Peak Hour		
		Total Trips ⁽¹⁾	Internal Trips	New Trips	Total Trips ⁽¹⁾	Internal Trips	New Trips	Total Trips ⁽¹⁾	Internal Trips	New Trips
Phase 1										
Single Family	141 du	61	-7	54	82	-11	72	79	-12	67
Condo/Townhouses	65 du	23	-5	18	28	-6	22	39	-8	31
Hotel	21 units	11	-3	8	13	-4	8	15	-5	10
Golf Course & Clubhouse ⁽²⁾	18 holes	34	-15	19	48	-21	28	57	-25	32
Total Phase 1		129	-29	100	171	-42	129	190	-49	141
Full Build-out										
Single Family	159 du	68	-8	60	91	-11	80	88	-13	75
Condo/Townhouses	65 du	23	-5	18	28	-7	21	39	-8	31
Hotel	21 units	11	-3	8	13	-5	8	15	-5	10
Golf Course & Clubhouse	18 holes	37	-16	21	53	-23	30	62	-27	35
Winery Restaurant ⁽³⁾	80 seats	2	0	2	21	0	21	14	0	14
Total Full Build-out		142	-32	110	205	-46	159	218	-53	165

Notes: (1) For Single-family and Condo/Townhouse trips, values shown reflect 50% of ITE values, plus internal trips to golf and hotel (subsequently subtracted, leaving rates for new traffic added to off-site roadways equivalent to 50% of ITE rates).

(2) Phase 1 Golf Course trips estimated to be 92% of full build-out trips as majority of golf trips will be from residential component which is not fully built in Phase 1.

(3) Midday Saturday Winery restaurant trips are 75% of Saturday Peak generator hour (evening) trips.



As indicated in Table 1, at full build-out, the project will generate 110 new trips during the AM peak hour, 159 new trips in the PM peak hour and 165 new trips during the Saturday midday peak hour. For Phase 1, the project is projected to generate approximately 14% less traffic than it will at full build-out.

The trips shown in Table 1 were distributed to the roadways and then added to the previously established 2017 No-Build volumes. It was assumed all trips from the southern/main parcel of the site would utilize the Main driveway and that the southern site driveway on Route 22 would be for emergency access and for access to the site's wastewater treatment plant and golf maintenance facility. Unsignalized intersection capacity analyses were conducted using Synchro software (version 8) for the Build condition during the PM peak hour (the critical time frame for the full build-out of the project). The analysis was performed at the Main site driveway on Route 22 assuming the existing geometry and a new northbound left-turn lane on Route 22. The results of this analysis indicate that the eastbound left turn exiting the driveway will operate at Level of Service (LOS) "E" with delays of 47.1 seconds and a volume to capacity ratio (v/c) of 0.40. All other movements will operate at LOS "B" or better.

After the completion of Phase 1, peak hour surveys will be conducted at the driveway to confirm that the average delay exiting the site does not fall below the projected LOS E. If the surveys indicate that excessive delays are experienced on the exiting movements, the Applicant will consider permitting residents to exit at the secondary (southern) access driveway. Appropriate permits will be filed at that time, if necessary.

2. Comment: It is our understanding that while at one time a right turn lane into the development was considered, it is no longer required or requested at the main entrance. This is acceptable and found to improve the projected operation of the left turn out (if justified) while not reducing the level of service on the RTE 22 main line.

Response: Comment noted.

3. Comment: At one time, it was understood that a fill operation was planned within the right-of-way at either side of the main entrance. This is not evident on the plans. A confirmation is requested.

Response: The area of fill proposed within the right-of-way is adjacent to the main entrance on Route 22, as shown in plan view on drawing GP-1, General Plan 1 and GP-2, General Plan 2, dated January 8, 2015. Cross sections have been taken at four locations at the main entrance, as shown on drawing



Miscellaneous Details 3, dated January 8, 2015. The applicant intends to import the necessary fill along Route 22 which will adhere to the local, state and federal requirements accordingly, however, if fill from within the project site is intended to be used, it will be testing to assure compliance with all applicable standards.

4. Comment: In reference to Drawings 4 and 6A, steep slopes are shown. The Vineyard Cottage drainage along RTE 44 is a concern as compounded by additional traffic generation. Accordingly, the north side of RTE 44 will require cutting back the slope, installing drainage facilities (closed system or ditch lines), and creation of a shoulder. The segment of improvement should begin at the proposed entrance to the Vineyard Cottage and continue to below the proposed wastewater plant and beyond.

Response: The proposed curbed road at the Vineyard Cottage and winery will intersect the flow from the uphill area thus reducing the contributing drainage area and runoff to Route 44. The increase of runoff from the proposed development will flow into the proposed closed drainage system and eventually drain into the proposed detention basin. The proposed detention basin will route and detain the flow to reduce the peak rate of flow ranging from 1-year to 100-year to match the existing flow rate before crossing Route 44. The proposed development at the Vineyard Cottage and Winery thus will not negatively impact Route 44. The applicant agrees to create a swale at the west side of Route 44 and downhill of the winery where grading is more accessible to further reduce the runoff from getting onto Route 44 and minimize potential environmental impacts to steep slopes.

5. Comment: Show sight distance triangles on the plans for all turning movements at all entrances. Provide a matrix at each location detailing required sight distance for each type of movement and available sight distance.

Response: Sight distance triangles and matrices indicating available and required sight distances at each location have been included on Drawings SD-1 and SD-2, dated January 8, 2015.

6. Comment: The proposed emergency entrance must not be a "Boulevard Entrance". It should be simply planned accommodating the design vehicle. A locked and gated access must clearly be shown on the plans. This emergency entrance must not be available for daily use of development occupants.

Response: The emergency entrance on Route 44 has been redesigned as requested and is shown as "gate with key card access for emergency access only" on the revised plans (please refer to Drawing GP-3, dated January 8, 2015).



7. Comment: The winery/residential/lookout entrance to RTE 44 must be detailed as a minor commercial entrance.

Response: The winery/residential/lookout entrance on Route 44 has been detailed as a minor commercial entrance (as indicated on drawing GP-4B, General Plan 4 – MDP, dated January 8, 2015).

8. Comment: The existing RTE 44 pull-off area should be closed with guiderail.

Response: The plan set has been updated to provide a guiderail at the Route 44 pull-off area (please refer to drawings GP-4A, General Plan 4 – Phase 1, and GP-4B, General Plan 4 – MDP, dated January 8, 2015).

9. Comment: All highway improvement drawings must show a labeled State Right-of-Way boundary.

Response: The State right-of-way boundary has been shown and labeled on all highway improvement drawings (sheets GP-1 through GP-6, General Plans 1-6, dated January 8, 2015).

10. Comment: Please offer confirmation that the gate shown on drawing GP-7 is on private lands.

Response: The gate shown on drawing GP-6 (formerly GP-7) is on private lands, as indicated by its location relative to the right-of-way boundary line on drawing GP-6, General Plan 6, dated January 8, 2015).

11. Comment: For drainage submissions please provide more detailed plans showing the watershed boundaries (with contours) and flow paths. The 11" x 17" plans provided in the SWPPP will not suffice.

Response: More detailed plans indicating the watershed boundaries (with contours) and flow paths have been provided (please refer to Existing Conditions Drainage Area Map, Fig 1 and Proposed Conditions Drainage Map, Fig 2, dated January 8, 2015).

12. Comment: All design storms must be considered (not only the 100 yr).

Response: The 1-, 5-, 10-, 25- and 50-year storms have been analyzed in addition to the 100-year storm (please refer to attachment E2 in SWPPP for Hydrocad analyses output).



13. Comment: Existing ponds or other features to be retained need to be described and included in the analysis.

Response: The existing ponds and other water features that are being retained as part of the Modified Project were labeled and modeled in drainage area maps and HydroCAD models. They were typically labeled as a “number within a triangle” symbol as shown in the legend of the drainage area maps. Pond D was labeled and modeled as “104A” in the drainage area maps as well as HydroCAD models.

14. Comment: Information on the culvert under RTE 22 at the southern end should be provided.

Response: Existing information for the 18” RCP pipe under Route 22 at the south entrance has been added to the plan, please refer to Drawing, GP-6.

15. Comment: Explain/show the drainage overflow (GP-6x).

Response: The grading downstream of the existing 24” pipe crossing Route 44 has been revised such that in the case of a drainage system failure, runoff will sheet flow downstream toward the golf course area without backing up onto the ROW.

16. Comment: Define the utility permit request operation in a separate application. If crossings are needed for private utilities, a Use & Occupancy permit may be required. If public utility services are intended, it is expected that the utility will apply for the permit. Please telephone or email for additional guidance.

Response: The utility permit request operation will be provided in a separate application. Public utility services are intended and the Applicant will coordinate with the utility provider on the permit application.

Project Development Guidance Comments

17. Comment: In reference to Drawing 3, there is concern about the proposed crossings under RTE 44 and depth relative to existing pipes. The ownership, depth, and condition of existing facilities must be confirmed.

Response: The wastewater treatment plant (“WWTP”) has been relocated to the south to the Golf Maintenance Facility area on the Harlem Valley Landfill Corp. property and the proposed crossings under Route 44 at that location have thus been removed. As such, the sheet formerly labeled as GP-3 has been removed. The new location of the Wastewater Treatment Plan can be seen on Drawing GP-6, dated January 8, 2015.



18. Comment: Detailed entrance plans must be submitted for each proposed entrance as design development advances.

Response: Detailed entrance plans for the main entrance and emergency entrances on Route 22 are provided on drawings GP-1, General Plan 1, GP-2, General Plan 2, and GP-6, General Plan 6, dated January 8, 2015. As the design development advances for the Route 44 entrances, plans will be forwarded to NYSDOT.

19. Comment: Specify guiderail end sections. It may not be appropriate to reuse existing.

Response: New Guide rail end sections are to be installed in accordance with NYSDOT Standards and have been specified on the plans, as indicated on sheets GP-4A, General Plan 4 Phase 1, GP-4B, General Plan 4 MDP, General Plan 5 Option 1, and General Plan 6, Option 2.

20. Comment: Signage is required westbound in addition to that shown on Dwg 5A.

Response: The appropriate westbound signage has been added to drawing 4A (formerly 5A).

21. Comment: One of the proposed Sugar Maples appears to be located within the clear distance (w/in 30' of the travel way). This tree should be moved so that is a minimum 30' from the white line.

Response: The tree has been relocated to be greater than 30 feet from the white line, as indicated on sheet GP-1, General Plan 1, dated January 8, 2015.

22. Comment: Align the detail for pavement thickness for work on State highways with standard details.

Response: The detail for pavement thickness for work on State highways has been aligned with the standard details, please refer to sheet MD-1, Miscellaneous Details 1, dated January 8, 2015.

23. Comment: In reference to Drawing 2, the condition of the large cross culvert on RTE 22 must be confirmed. This culvert is located north of the proposed main entrance on RTE 22.

Response: VHB performed an inspection of the large cross culvert under Route 22 in December 2014. Please refer to VHB Memorandum dated January 13, 2015, (Attachment B), which provides a detailed assessment of the condition of the culvert. As indicated in the Memo, the structure is in overall fair condition, with some areas exhibiting spalling. The structure was last inspected by



NYSDOT on October 24, 2013 when it received a condition rating of 5.29 on a scale of seven (7.0), with seven being new or like new construction.

24. Comment: The Applicant shall satisfactorily complete the Smart Growth Prescreening Tool required under the NYS Smart Growth Public Infrastructure Policy Act (SGPIPA).

Response: The Smart Growth Prescreening Tool has been completed – please refer to Attachment A.

25. Comment: This project is subject to the requirements of the State's Drivers First initiative. Delay to the traveling public must be minimized.

Response: Comment noted. During construction, delays to the traveling public on Routes 22 and 44 will be minimized to the extent possible. Construction activity that would impact the flow of traffic will be scheduled so as to avoid impacts during the peak travel periods, typically the weekday AM and PM commuter periods.

26. Comment: The Applicant's resubmission should include enumerated comments noting sheet, date, detail no., etc. Each revision is to be clouded (or otherwise called out) with a revision number. Furthermore, any substantive revisions not related to NYSDOT comments must be clouded (or otherwise called out) and noted in the re-submittal letter.

Response: The response contained herein are numbered in accordance with the comment letter and references to sheet number, date, detail number, etc. are provided where appropriate. Each revision, including revisions for substantive non-NYSDOT comments, has been clouded and labeled with a revision number.

27. Comment: Future submissions shall be electronic (PDF) with two paper copies to the Poughkeepsie office and one paper copy to the Middletown office of the following:

Regional Permit Coordinator
NYS Department of Transportation
4 Burnett Boulevard
Poughkeepsie, NY 12603

Permit Engineer, residency 8-2
NYS Department of Transportation
334 Violet Avenue
Poughkeepsie, NY 12601

Response: Comment noted.

Michael Sassi, P.E.
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Page 9DOT



Sincerely,

A handwritten signature in black ink that reads "Amanda DeCesare".

Amanda DeCesare, PE
Senior Project Manager
adecesare@vhb.com

CC: Chairman Fontaine and Members of the Planning Board;
David Everett, Esq.;
Peter Wise, Esq.; and
Pedro Torres

Michael Sassi, P.E.
Ref: 29011.00
January 8, 2015



Attachment A:
Smart Growth Screening Tool

Smart Growth Screening Tool

PIN

Prepared By:

Smart Growth Screening Tool (STEP 1)

NYSDOT & Local Sponsors – Fill out the Smart Growth Screening Tool until the directions indicate to **STOP** for the project type under consideration. For all other projects, complete answering the questions. For any questions, refer to [Smart Growth Guidance](#) document.

Title of Proposed Project:

Location of Project: Amenia, Dutchess Co., NY

Brief Description: Construction of Driveways and a left-turn lane on NYS 22 to support the redevelopment of an existing golf course with the addition of a 215-unit vacation community.

A. Infrastructure:

Addresses SG Law criterion a. –

(To advance projects for the use, maintenance or improvement of existing infrastructure)

1. Does this project use, maintain, or improve existing infrastructure?

Yes

No

N/A

Explain: (use this space to expand on your answers above – the form has no limitations on the length of your narrative)

Uses existing roads for access and existing power lines for electricity supply

Maintenance Projects Only

a. Continue with screening tool for the four (4) types of maintenance projects listed below, as defined in **NYSDOT PDM Exhibit 7-1 and described in 7-4:**

<https://www.dot.ny.gov/divisions/engineering/design/dqab/pdm>

- Shoulder rehabilitation and/or repair;
- Upgrade sign(s) and/or traffic signals;

Smart Growth Screening Tool

- Park & ride lot rehabilitation;
 - 1R projects that include single course surfacing (inlay or overlay), per Chapter 7 of the NYSDOT Highway Design Manual.
- b. For all other maintenance projects, **STOP here**. Attach this document to the programmatic [Smart Growth Impact Statement and signed Attestation](#) for Maintenance projects.

For all other projects (**other than maintenance**), continue with screening tool.

B. Sustainability:

NYSDOT defines Sustainability as follows: A sustainable society manages resources in a way that fulfills the community/social, economic and environmental needs of the present without compromising the needs and opportunities of future generations. A transportation system that supports a sustainable society is one that:

- Allows individual and societal transportation needs to be met in a manner consistent with human and ecosystem health and with equity within and between generations.
- Is safe, affordable, and accessible, operates efficiently, offers choice of transport mode, and supports a vibrant economy.
- Protects and preserves the environment by limiting transportation emissions and wastes, minimizes the consumption of resources and enhances the existing environment as practicable.

For more information on the Department's Sustainability strategy, refer to Appendix 1 of the Smart Growth Guidance and the NYSDOT web site, www.dot.ny.gov/programs/greenlites/sustainability

(Addresses SG Law criterion j : to promote sustainability by strengthening existing and creating new communities which reduce greenhouse gas emissions and do not compromise the needs of future generations, by among other means encouraging broad based public involvement in developing and implementing a community plan and ensuring the governance structure is adequate to sustain and implement.)

1. Will this project promote sustainability by strengthening existing communities?

Yes No N/A

2. Will the project reduce greenhouse gas emissions?

Yes No N/A

Explain: (use this space to expand on your answers above)

Overall project will strengthen the Town of Amenia by increasing the tax base and spending in the Town. Providing a vacation community at the end of the commuter rail line from New York City will reduce greenhouse gases which would otherwise be emitted by vacations spent further afield and which do not have access to mass transit. Roadway improvements and driveways will allow the development to operate satisfactorily.

Smart Growth Screening Tool

C. Smart Growth Location:

Plans and investments should preserve our communities by promoting its distinct identity through a local vision created by its citizens.

(Addresses SG Law criteria b and c: to advance projects located in municipal centers; to advance projects in developed areas or areas designated for concentrated infill development in a municipally approved comprehensive land use plan, local waterfront revitalization plan and/or brownfield opportunity area plan.)

1. Is this project located in a developed area?

Yes No N/A

2. Is the project located in a municipal center?

Yes No N/A

3. Will this project foster downtown revitalization?

Yes No N/A

4. Is this project located in an area designated for concentrated infill development in a municipally approved comprehensive land use plan, waterfront revitalization plan, or Brownfield Opportunity Area plan?

Yes No N/A

Explain: (use this space to expand on your answers above)

Improved tax base and influx of families on weekend getaways supported by this project will contribute to the revitalization of the Town's central business district.

D. Mixed Use Compact Development:

Future planning and development should assure the availability of a range of choices in housing and affordability, employment, education transportation and other essential services to encourage a jobs/housing balance and vibrant community-based workforce.

(Addresses SG Law criteria e and i: to foster mixed land uses and compact development, downtown revitalization, brownfield redevelopment, the enhancement of beauty in public spaces, the diversity and affordability of housing in proximity to places of employment, recreation and commercial

Smart Growth Screening Tool

development and the integration of all income groups; to ensure predictability in building and land use codes.)

1. Will this project foster mixed land uses?
Yes No N/A
2. Will the project foster brownfield redevelopment?
Yes No N/A
3. Will this project foster enhancement of beauty in public spaces?
Yes No N/A
4. Will the project foster a diversity of housing in proximity to places of employment and/or recreation?
Yes No N/A
5. Will the project foster a diversity of housing in proximity to places of commercial development and/or compact development?
Yes No N/A
6. Will this project foster integration of all income groups and/or age groups?
Yes No N/A
7. Will the project ensure predictability in land use codes?
Yes No N/A
8. Will the project ensure predictability in building codes?
Yes No N/A

Explain: (use this space to expand on your answers above)

Overall project is being designed to preserve and enhance the beauty of the valley, including the addition of a scenic overlook. Proposed highway improvements will comply with all applicable codes, thereby supporting their continued predictability.

E. Transportation and Access:

NYSDOT recognizes that Smart Growth encourages communities to offer a wide range of transportation options, from walking and biking to transit and automobiles, which increase people's access to jobs, goods, services, and recreation.

(Addresses SG Law criterion f: to provide mobility through transportation choices including improved public transportation and reduced automobile dependency.)

Smart Growth Screening Tool

1. Will this project provide public transit?

Yes No N/A

2. Will this project enable reduced automobile dependency?

Yes No N/A

3. Will this project improve bicycle and pedestrian facilities (such as shoulder widening to provide for on-road bike lanes, lane striping, crosswalks, new or expanded sidewalks or new/improved pedestrian signals)?

Yes No N/A

(Note: Question 3 is an expansion on question 2. The recently passed Complete Streets legislation requires that consideration be given to complete street design features in the planning, design, construction, reconstruction and rehabilitation, but not including resurfacing, maintenance, or pavement recycling of such projects.)

Explain: (use this space to expand on your answers above)

Redevelopment project will provide a shuttle for residents and employees to the train station.

F. Coordinated, Community-Based Planning:

Past experience has shown that early and continuing input in the transportation planning process leads to better decisions and more effective use of limited resources. For information on community based planning efforts, the MPO may be a good resource if the project is located within the MPO planning area.

(Addresses SG Law criteria g and h: to coordinate between state and local government and inter-municipal and regional planning; to participate in community based planning and collaboration.)

1. Has there been participation in community-based planning and collaboration on the project?

Yes No N/A

2. Is the project consistent with local plans?

Yes No N/A

3. Is the project consistent with county, regional, and state plans?

Yes No N/A

Smart Growth Screening Tool

4. Has there been coordination between inter-municipal/regional planning and state planning on the project?

Yes No N/A

Explain: (use this space to expand on your answers above)

Overall project has gone through the SEQRA process with public input and coordination with all applicable jurisdictional agencies.

G. Stewardship of Natural and Cultural Resources:

Clean water, clean air and natural open land are essential elements of public health and quality of life for New York State residents, visitors, and future generations. Restoring and protecting natural assets, and open space, promoting energy efficiency, and green building, should be incorporated into all land use and infrastructure planning decisions.

(Addresses SG Law criterion d :To protect, preserve and enhance the State’s resources, including agricultural land, forests surface and ground water, air quality, recreation and open space, scenic areas and significant historic and archeological resources.)

1. Will the project protect, preserve, and/or enhance agricultural land and/or forests?

Yes No N/A

2. Will the project protect, preserve, and/or enhance surface water and/or groundwater?

Yes No N/A

3. Will the project protect, preserve, and/or enhance air quality?

Yes No N/A

4. Will the project protect, preserve, and/or enhance recreation and/or open space?

Yes No N/A

5. Will the project protect, preserve, and/or enhance scenic areas?

Yes No N/A

6. Will the project protect, preserve, and/or enhance historic and/or archeological resources?

Yes No N/A

Explain: (use this space to expand on your answers above)

Smart Growth Screening Tool

Overall project will employ best practices water management, will preserve scenic beauty of the golf course and the valley. Development will respect existing archaeological resources.

Smart Growth Screening Tool

Smart Growth Impact Statement (STEP 2)

NYSDOT: Complete a Smart Growth Impact Statement (SGIS) below using the information from the Screening Tool.

Local Sponsors: The local sponsors are **not** responsible for completing a Smart Growth Impact Statement. Proceed to **Step 3**.

Smart Growth Impact Statement

PIN:

Project Name:

Pursuant to ECL Article 6, this project is compliant with the New York State Smart Growth Public Infrastructure Policy Act. This project has been determined to meet the relevant criteria, to the extent practicable, described in ECL Sec. 6-0107. Specifically, the project:

-
-
-
-
-
-

This publically supported infrastructure project complies with the state policy of maximizing the social, economic and environmental benefits from public infrastructure development. The project will not contribute to the unnecessary costs of sprawl development, including environmental degradation, disinvestment in urban and suburban communities, or loss of open space induced by sprawl.

Smart Growth Screening Tool

Review & Attestation Instructions (STEP 3)

Local Sponsors: Once the Smart Growth Screening Tool is completed, the next step is to submit the project certification statement (**Section A**) to Responsible Local Official for signature. After signing the document, the completed Screening Tool and Certification statement should be sent to NYSDOT for review as noted below.

NYSDOT: For state-let projects, the Screening Tool and SGIS is forwarded to Regional Director/ RPPM/Main Office Program Director or designee for review, and upon approval, the attestation is signed (**Section B.2**). For locally administered projects, the sponsor's submission and certification statement is reviewed by NYSDOT staff, the appropriate box (**Section B.1**) is checked, and the attestation is signed (Section B.2).

A. CERTIFICATION (LOCAL PROJECT)

I HEREBY CERTIFY, to the best of my knowledge, all of the above to be true and correct.

Preparer of this document:

Signature

Date

Title

Printed Name

Responsible Local Official (for local projects):

Signature

Date

Title

Printed Name

Smart Growth Screening Tool

B. ATTESTATION (NYSDOT)

1. I HEREBY:

Concur with the above certification, thereby attesting that this project is in compliance with the State Smart Growth Public Infrastructure Policy Act

Concur with the above certification, with the following conditions (information requests, confirming studies, project modifications, etc.):

(Attach additional sheets as needed)

do not concur with the above certification, thereby deeming this project ineligible to be a recipient of State funding or a subrecipient of Federal funding in accordance with the State Smart Growth Public Infrastructure Policy Act.

2. **NOW THEREFORE**, pursuant to ECL Article 6, this project is compliant with the New York State Smart Growth Public Infrastructure Policy Act, to the extent practicable, as described in the attached Smart Growth Impact Statement.

NYSDOT Commissioner, Regional Director, MO Program Director,
Regional Planning & Programming Manager (or official designee):

Signature

Date

Title

Printed Name

Michael Sassi, P.E.
Ref: 29011.00
January 8, 2015



Attachment B:
Culvert Inspection



To: Michael Dignacco

Date: January 13, 2015

Memorandum

Project #: 29011.00

From: Kevin Powers, P.E.

Re: Culvert Inspection - Route 22 Amenia New York

On December 19, 2014, VHB performed a condition assessment inspection of a 3-sided culvert structure (BIN C824024) which carries New York Route 22 over Amenia Stream in the town of Amenia. The purpose of the inspection was to measure and record primary dimensions of the structure, document the condition of the structure, and identify any structural deficiencies. The structure was last inspected by NYSDOT on October 24, 2013 and received a condition rating of 5.29 on a scale of seven (7), with seven being new or like new construction.

Observations

New York Route 22 over Amenia Stream carries two travel lanes, striped 11' (southbound) and 12' (northbound) with a 5' shoulder on each side. The overall structure length is 41'-0" from fascia to fascia (Figure 1, Photo 1, and Photo 2). The structure has box beam guide railing which continues off the structure for an unspecified distance. The 3-sided culvert structure has a clear opening 14'-0" +/- and an average clear height to the stream bed of 9'-5" on the west face (Figure 2, Photo 8) and 10'-3½" on the east face (Figure 3, Photo 11). The top slab measured 21" +/- with a 3" chamfer at the interface with the vertical legs. The wingwall lengths vary in length and have a nominal thickness of 18".

Northwest Wingwall

The northwest wingwall (Photo 9) is 12'-6" in length and has an exposed height ranging from 10'-9" to 0'-6" at the free end (Figure 2). The overall condition is fair with spalling along the top edge of the wall and at the joint interface with the culvert.

Southwest Wingwall

The southwest wingwall (Photo 10) is 9'-3" in length and has an exposed height ranging from 10'-7" to 3'-11" at the free end (Figure 2). The overall condition is fair with spalling along the top edge of the wall and at the free end. The wall also exhibited scaling of the concrete on the upper half of the exposed face and some efflorescence at the interface with the culvert.

Northeast Wingwall

The northeast wingwall (Photo 12) is 11'-3" in length and has an exposed height ranging from 11'-10" to 4'-5" at the free end (Figure 3). The overall condition is fair with spalling along half of the top edge of the wall.

Southeast Wingwall

The southeast wingwall (Photo 13) is 13'-3" in length and has an exposed height ranging from 12'-3" to 3'-4" at the free end (Figure 3). The overall condition is fair with spalling along the top edge of the wall.

Culvert Vertical Legs

100 Great Oaks Boulevard
Suite 118
Albany, NY 12203
P 518.389.3600



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Page 2

Memorandum

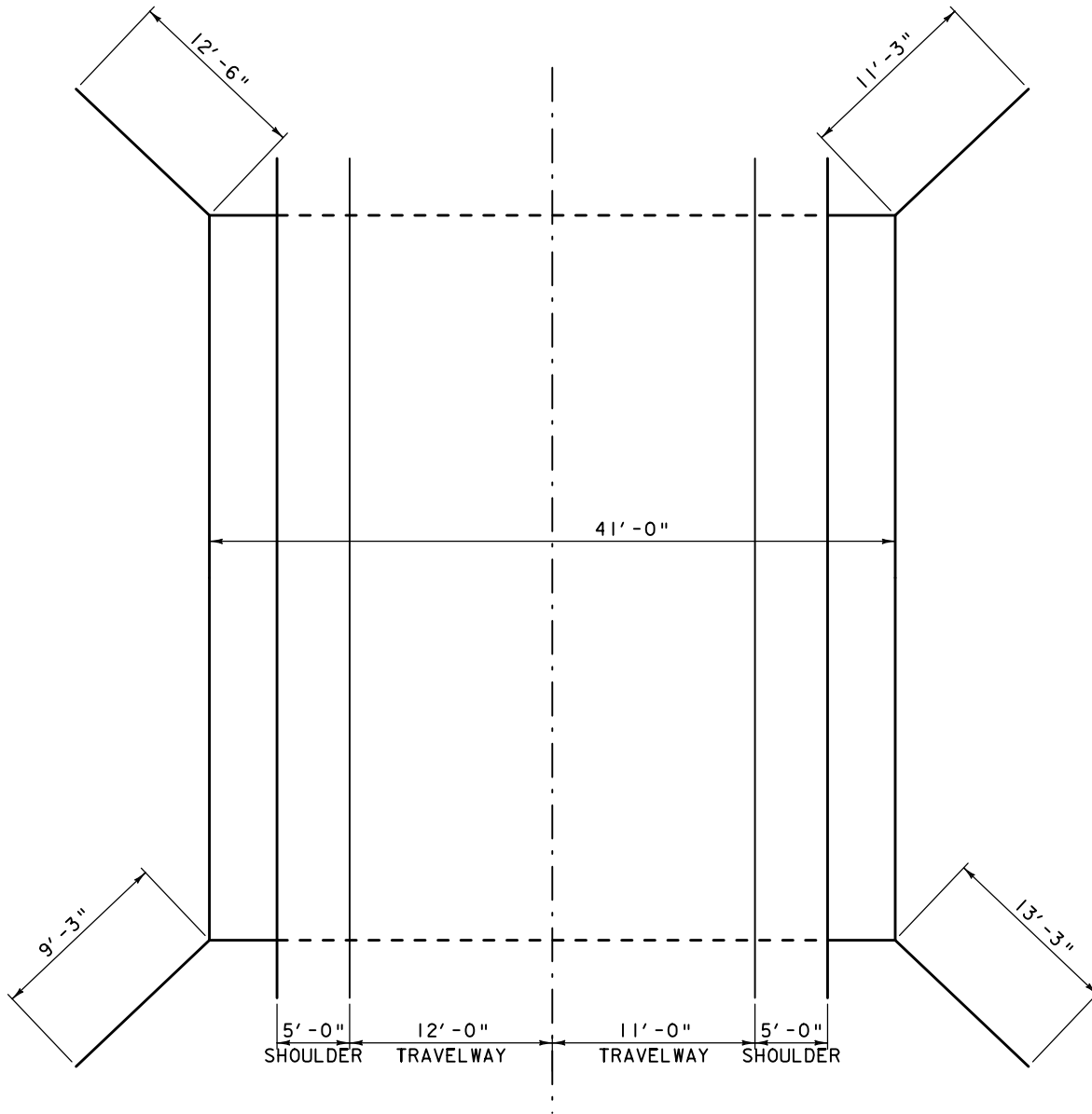
The vertical legs of the culvert are in good condition, however vertical hairline cracks were observed on each leg. Along the north leg (Photo 16 and Photo 17) the hairline cracks were measured at 9'-5", 17'-3", 24'-10" and 31'-10" from the west end of the culvert. Along the south leg (Photo 18 and Photo 19) hairline cracks were measured at 11'-6", 18'-3", and 28'-8" from the west end of the culvert. The concrete was sounded along each crack and was determined to be solid and in good condition.

Culvert Top Slab

The fascias of the culvert top slab are in poor condition (Photo 14, west side, Photo 15, east side) with significant spalling and exposed rebar along the entire fascia length. The remainder of the culvert top slab under the roadway (Photo 20 and Photo 21) is in good condition.

FIGURES

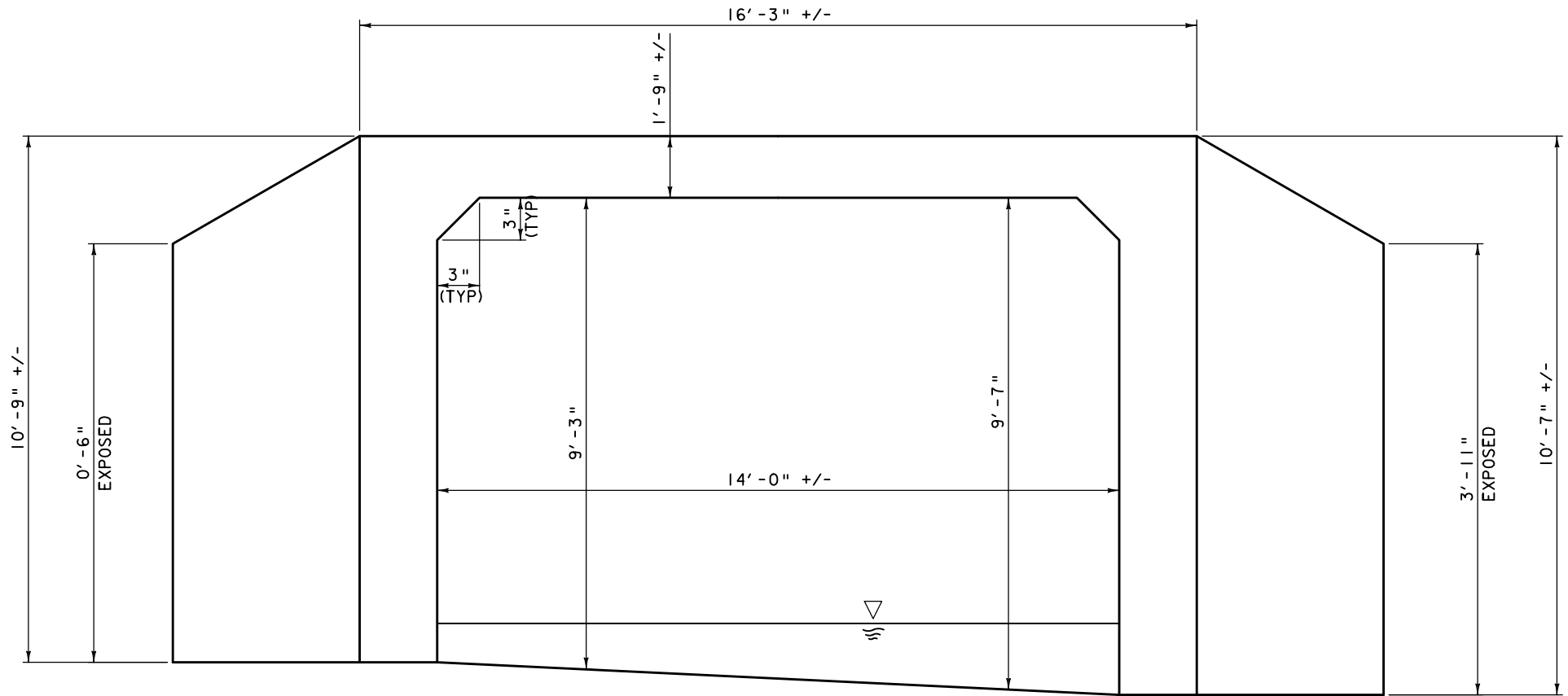
GRID
NORTH



PLAN VIEW
NTS



PROJECT NAME:	SILO RIDGE ROUTE 22 CULVERT INSPECTION
FILE NAME:	Plan View.dgn
PROJECT LEADER:	K. POWERS
PLOT DATE:	1/12/2015
DRAWN BY:	A.J.D.
CHECKED BY:	K.B.P.
FIGURE	1

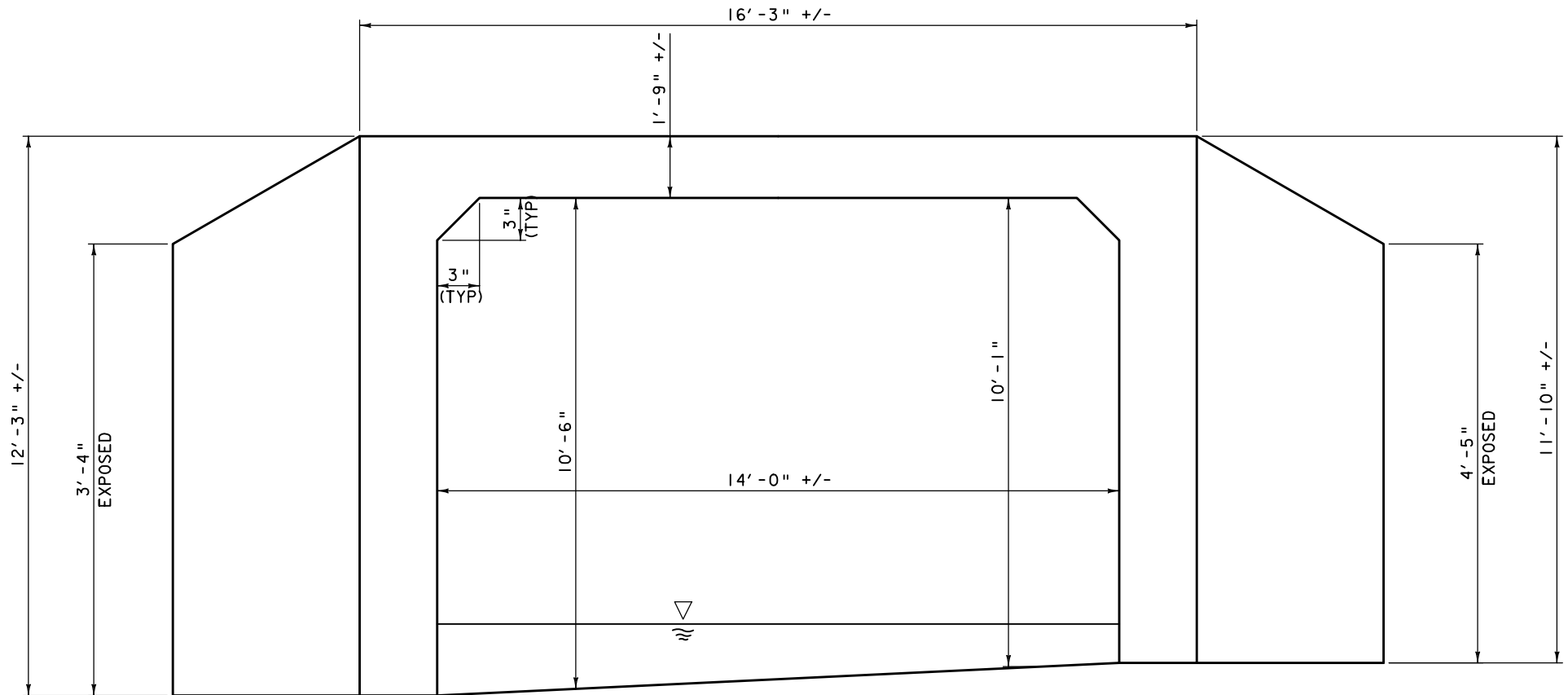


WEST FACE ELEVATION

(NTS)



PROJECT NAME:	SILO RIDGE
PROJECT NUMBER:	ROUTE 22 CULVERT INSPECTION
FILE NAME:	West Fascia Culvert Elevation PL 05.dwg
PROJECT LEADER:	K. POWERS
DATE:	1/13/2015
DRAWN BY:	A.J.D.
CHECKED BY:	K.B.P.
FIGURE:	FIGURE 2



EAST FACE ELEVATION

(NTS)



PROJECT NAME:	SILO RIDGE
PROJECT NUMBER:	ROUTE 22 CULVERT INSPECTION
FILE NAME:	East Fascia Culvert Elevation PLW.dwg
PROJECT LEADER:	K. POWERS
DRAWN BY:	A.J.D.
CHECKED BY:	K.B.P.
FIGURE	3

PHOTOS

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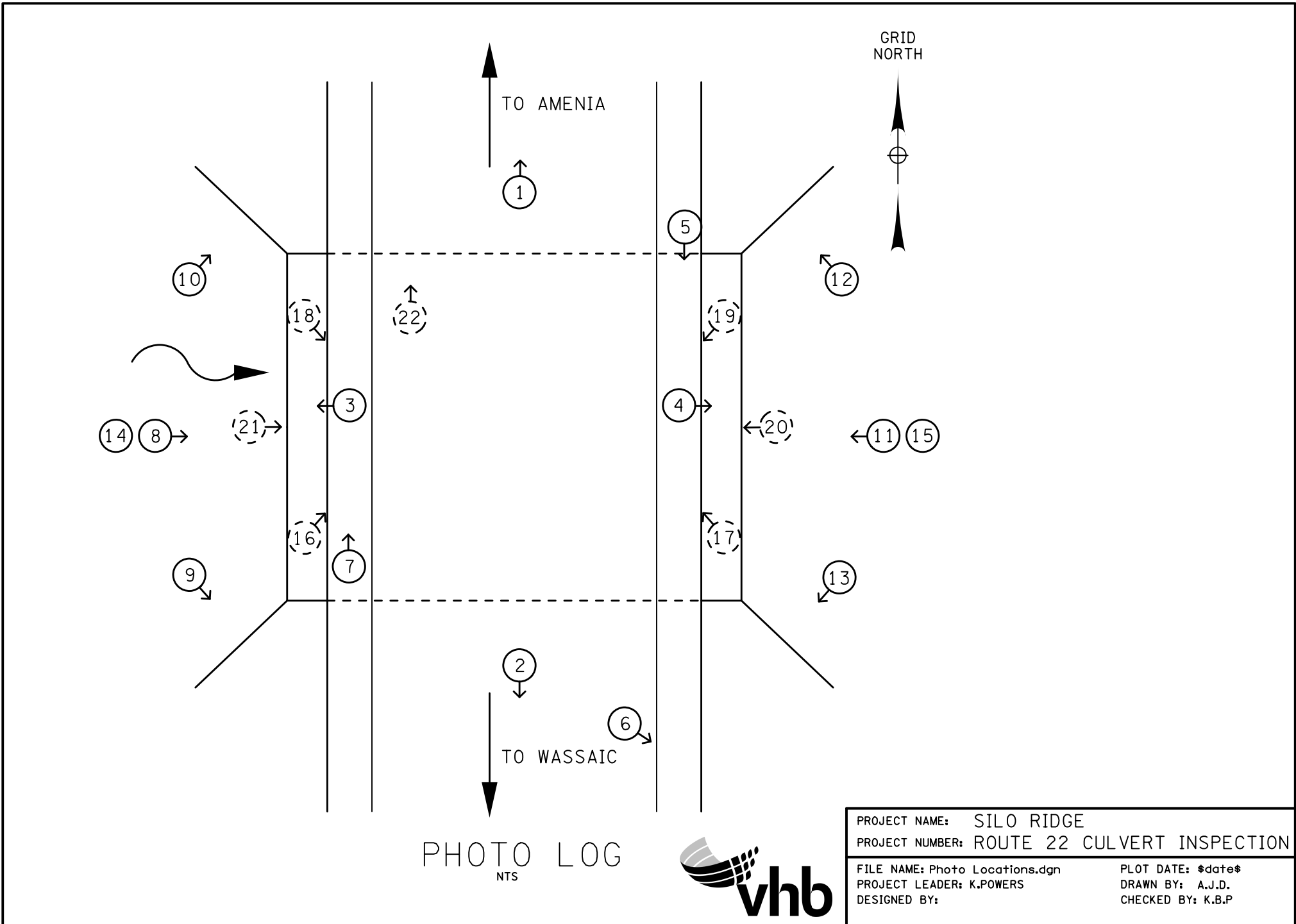




Photo 1: Route 22 (looking north)



Photo 2: Route 22 (looking south)



Photo 3: Upstream (looking west)



Photo 4: Downstream (looking east)



Photo 5: East Bridge Rail (looking south)



Photo 6: East Bridge Rail, Minor Damage



Photo 7: West Bridge Rail (looking north)



Photo 8: Box Culvert – West Fascia



Photo 9: Northwest Wingwall



Photo 10: Southwest Wingwall



Photo 11: Box Culvert – East Fascia



Photo 12: Northeast Wingwall



Photo 13: Southeast Wingwall



Photo 14: Exposed Rebar, West Fascia Top Slab



Photo 15: Exposed Rebar, East Fascia Top Slab



Photo 16: North Leg (looking east)



Photo 17: North Leg (looking west)



Photo 18: South Leg (looking east)



Photo 19: South Leg (looking west)



Photo 20: Top Slab (looking west)



Photo 21: Top Slab (looking east)



Photo 22: Hairline Crack, typical

NYS DOT Ref No. AW 08.05.701; SEQR # 07-0192

Date Issued: 7/31/2014

Latest Issue: 1/8/2015

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GP-4A	General Plan 4 - Phase 1	1/8/2015
GP-4B	General Plan 4 - MDP	1/8/2015
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SD-1	Sight Distance Triangles 1	1/8/2015
SD-2	Sight Distance Triangles 2	1/8/2015

General Notes:

REFERENCES

- SPECIFICATIONS: "NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYS DOT) CONSTRUCTION AND MATERIALS", OFFICE OF ENGINEERING, LATEST EDITION.
- "NEW YORK STATE DEPARTMENT OF TRANSPORTATION HIGHWAY DESIGN MANUAL" VOLUMES I & II, LATEST EDITION.
- "AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS", 2011.
- NATIONAL MUTCD (2009) EDITION AND THE NEW YORK STATE SUPPLEMENT.
- AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES (ADAAG), LATEST EDITION.

GENERAL

- THE LOCAL FIRE DEPARTMENTS, EMERGENCY SQUADS, AND THE LOCAL POLICE DEPARTMENT SHALL BE NOTIFIED A MINIMUM OF FIVE (5) WORKING DAYS PRIOR TO THE CONTRACTOR COMMENCING CONSTRUCTION. THE ABOVE LISTED AGENCIES OR DEPARTMENTS SHALL BE NOTIFIED A MINIMUM OF ONE (1) WORKING DAY IN ADVANCE OF THE CONTRACTOR CONDUCTING WORK THAT WILL HAVE AN IMPACT ON THE TRAVEL ROUTES OR RESPONSE ROUTES.
- ALL NECESSARY PERMITS SHALL BE OBTAINED PRIOR TO THE COMMENCEMENT OF WORK.
- ALL WORK WITHIN THE N.Y.S. RIGHT-OF-WAY (ROW), AS SHOWN ON THE PLANS, SHALL CONFORM TO THE NYS DOT STANDARD SPECIFICATION, STANDARD DETAILS, AND PERMITS, OR AS ORDERED BY THE NYS DOT REPRESENTATIVE.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT, DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE THE CONTRACTOR TO PERFORM WORK IN ACCORDANCE WITH FIELD CONDITIONS.
- THE CONTRACTOR SHALL VISIT THE PROJECT SITE BEFORE BIDDING TO BECOME FAMILIAR WITH PRESENT CONDITIONS AND TO JUDGE FOR THEMSELVES THE EXTENT AND NATURE OF WORK TO BE DONE UNDER THIS CONTRACT.
- THE CONTRACTOR SHALL TAKE PRECAUTIONS NOT TO LEAVE DEBRIS, MATERIALS AND TOOLS, ETC. ON THE ROADWAY SURFACE WHEN LEAVING THE WORK AREA. IN ADDITION, THE CONTRACTOR SHALL TAKE PRECAUTIONS THAT NO DEBRIS, ETC., INTERFERES WITH ADJACENT OPEN TRAFFIC LANES AND PEDESTRIAN WALKS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS, FEES AND PERMITS TO COMPLETE THE WORK.
- REMOVE, RESTORE OR REPLACE ALL EXISTING SIGNS AS ORDERED BY STATE ENGINEER.
- ALL WORK AT SITE ACCESS POINTS SHALL BE COORDINATED WITH THE APPROVED SITE PLAN.
- THE CONTRACTOR SHALL CLEAN ALL EXISTING CATCH BASINS ALONG AND IMMEDIATELY ADJACENT TO ROUTE 22 AND ROUTE 44 SITE FRONTAGE AT THE COMPLETION OF CONSTRUCTION AS DIRECTED BY THE STATE ENGINEER.

- THE CONTRACTOR SHALL REPAIR THE EXISTING SHOULDER, SIDEWALK AND CURBING WITHIN THE PROJECT LIMITS, AS ORDERED BY THE STATE ENGINEER.

UTILITIES

- THE EXISTING UTILITY INFORMATION SHOWN ON THE PLANS WAS DERIVED SOLELY FROM PLOTTING VISIBLE ABOVE GROUND UTILITY FEATURES. CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES FOR MARK-OUT OF ALL FACILITIES PRIOR TO CONSTRUCTION. THE UTILITY COMPANIES SHALL BE NOTIFIED A MINIMUM OF FIVE (5) WORKING DAYS BEFORE THE CONTRACTOR COMMENCES WORK. ANY UTILITY RELOCATIONS OR CROSSINGS SHALL BE COORDINATED WITH THE UTILITY COMPANY OWNING THE FACILITY.
- EXISTING UTILITY FACILITIES REMOVAL, RELOCATION OR MODIFICATION SHALL BE COMPLETED PRIOR TO COMMENCING PAVEMENT WORK. CONTRACTOR IS RESPONSIBLE FOR THE COORDINATION OF THE MODIFICATION OF ALL UTILITIES.

SURVEY

- THE CONTRACTOR IS TO EMPLOY A LICENSED SURVEYOR TO PERFORM ANY REQUIRED LAYOUT, ESTABLISHMENT OF ELEVATIONS AND GRADES, PRIOR TO CONSTRUCTION. ALL LAYOUT OF PROPOSED WORK SHALL BE DONE IN CONFORMANCE WITH ITEM 625.01 - SURVEY AND STAKEOUT.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NOT TO DISTURB ANY MONUMENTS AND BENCH MARKS WITHIN PROJECT LIMITS PRIOR TO THE START OF ANY EXCAVATION.
- ANY EXPENSE INCURRED IN REPLACING MONUMENTS OR BENCHMARKS THAT THE CONTRACTOR, OR ANY EMPLOYEE, MAY HAVE FAILED TO PRESERVE SHALL BE CHARGED TO THE CONTRACTOR FROM THE AMOUNT TO BE PAID FOR DOING THE WORK UNDER THIS CONTRACT.

ROADWAY IMPROVEMENT PLANS

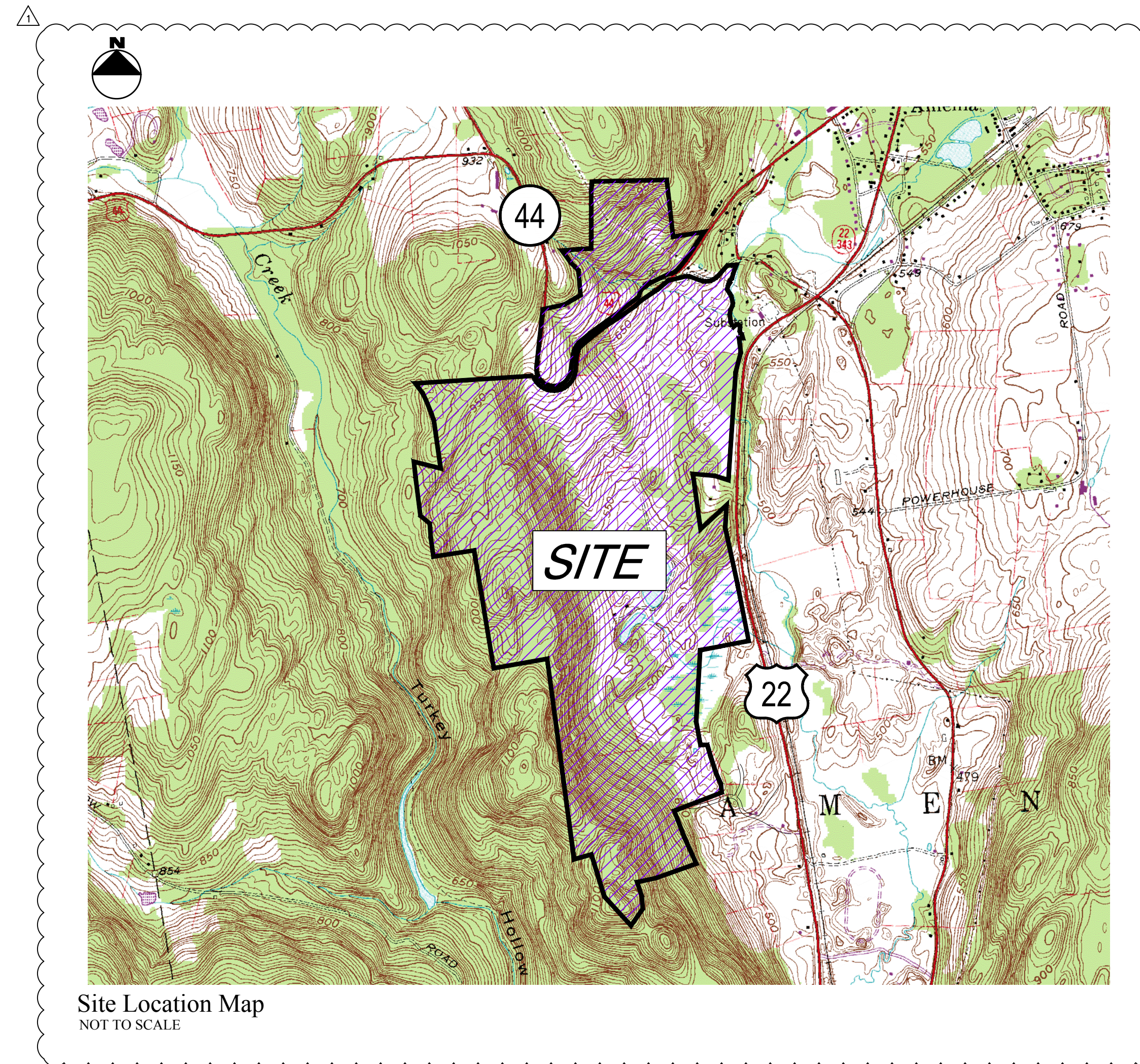
ON

US ROUTE 44 AND NYS ROUTE 22

AT SILO RIDGE

BETWEEN REFERENCE MARKERS

AMENIA, NEW YORK



REFERENCE DRAWINGS:

- SILO RIDGE RESORT COMMUNITY PHASE 1 SITE PLANS
- C2.00 - OVERALL EXISTING CONDITIONS PLAN
 - C4.00 - OVERALL SITE LAYOUT PLAN
 - C12.00 - OVERALL EROSION AND SEDIMENT CONTROL PLAN
 - C12.02 - EROSION AND SEDIMENT CONTROL PLAN 2
 - C12.03 - EROSION AND SEDIMENT CONTROL PLAN 3
 - C12.09 - EROSION AND SEDIMENT CONTROL PLAN 11
 - C12.10 - OVERALL TEMPORARY SEDIMENT BASIN PLAN
 - C12.11 - TEMPORARY SEDIMENT BASIN PLAN 1
 - C12.12 - TEMPORARY SEDIMENT BASIN PLAN 2
 - C12.13 - TEMPORARY SEDIMENT BASIN PLAN 3
 - C12.14 - TEMPORARY SEDIMENT BASIN PLAN 4
 - L3.02 - PLANTING PLAN TILE 2
 - L3.03 - PLANTING PLAN TILE 3
 - L3.10 - PLANTING PLAN TILE 10
 - L3.13 - PLANTING PLAN ENLARGEMENT
 - L3.14 - PLANTING PLAN ENLARGEMENT
 - L3.20 - OFF SITE SCREENING IMPROVEMENTS

- SILO RIDGE RESORT COMMUNITY MASTER DEVELOPMENT PLANS
- SP-2 - OVERALL SITE PLAN
 - SP-5 - OVERALL PHASING PLAN
 - GP-1 - GRADING PLAN 1
 - GP-2 - GRADING PLAN 2
 - SW-1 - OVERALL STORMWATER MANAGEMENT PRACTICE IDENTIFICATION PLAN
 - U-1 - OVERALL WASTEWATER MASTER PLAN
 - U-2 - OVERALL WATER SUPPLY SYSTEM MASTER PLAN

NYS DOT REFERENCE STANDARD SHEETS:

- 209: SOIL EROSION AND SEDIMENT CONTROL
 - 209-01 - LINEAR MEASURES
 - 209-02 - CHECK DAMS
 - 209-03 - DRAINAGE STRUCTURE INLET PROTECTION
 - 209-04 - PIPE INLET/OUTLET PROTECTION - PIPE SLOPE DRAIN
 - 209-05 - CONSTRUCTION ENTRANCES
- 604: DRAINAGE STRUCTURES
 - 604-02 - DRAINAGE STRUCTURE DETAILS
- 606: GUIDE RAILING
 - 606-04 - BOX BEAM GUIDE RAIL
- 611: PLANTING
 - 611-01 - LANDSCAPE PLANTING DETAILS
- 655: FRAMES, GRATES AND COVERS
 - 655-01 - RECTANGULAR GRATES
 - 655-02 - PARALLEL BAR FRAMES AND GRATES
- 685: PAVEMENT MARKINGS
 - 685-01 - PAVEMENT MARKING DETAILS



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SPECIFICATIONS NOTE: NYS DOT ITEMS INDICATED WITHIN THE PLANS ARE FROM THE LATEST EDITION OF THE SPECIFICATIONS, AND ARE REFERENCED FOR MATERIALS AND METHODS OF CONSTRUCTION.

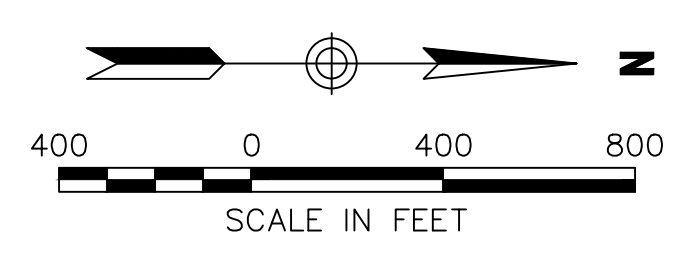
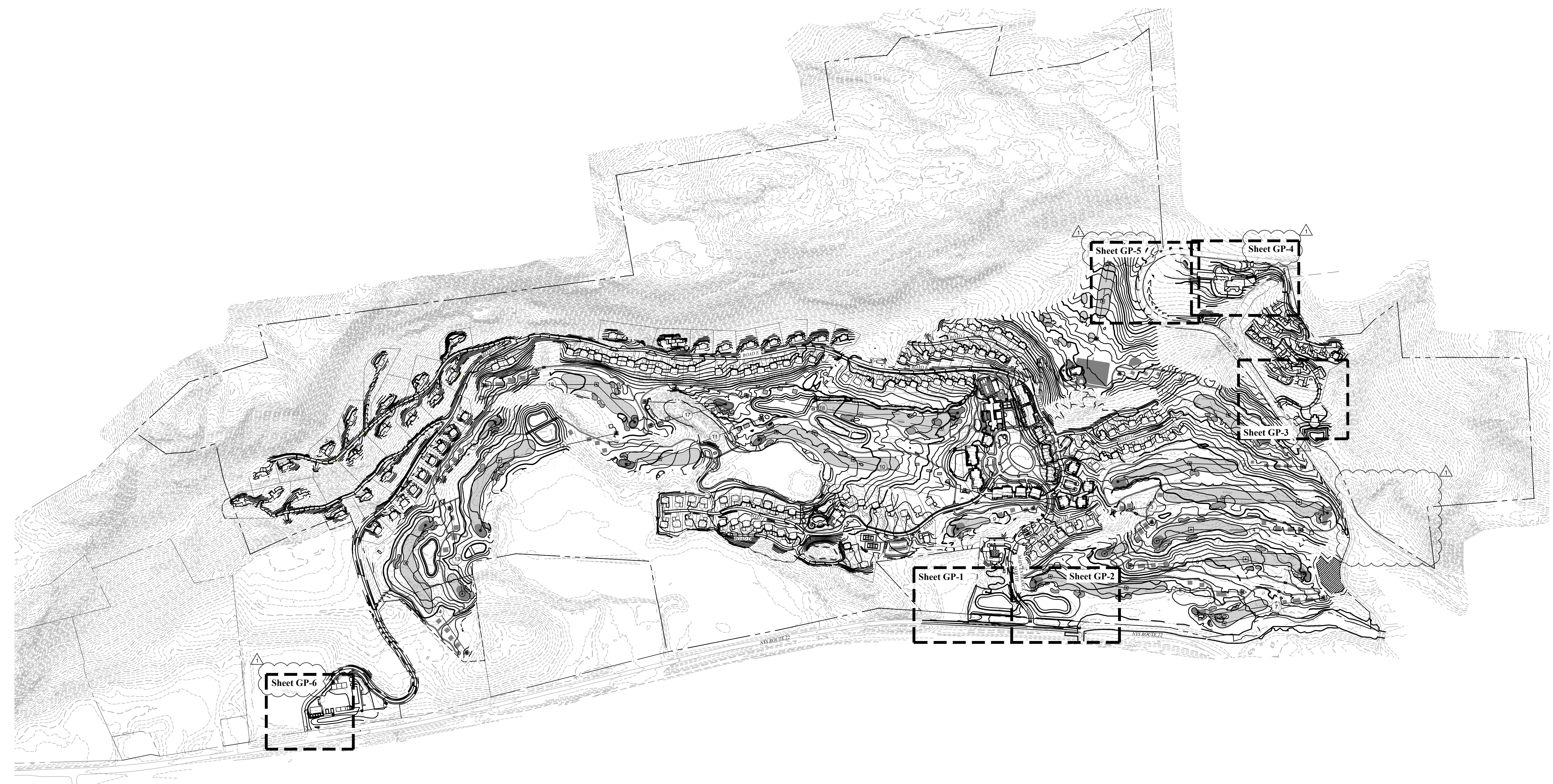
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CAD checked by *MB* Approved by *ACD*
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Project Title

Silo Ridge Resort Community

4651 Route 22
Amenia, New York

Issued for
NYS DOT Permit

Drawing Title
Overall Key Plan

Drawing Number
GP-0

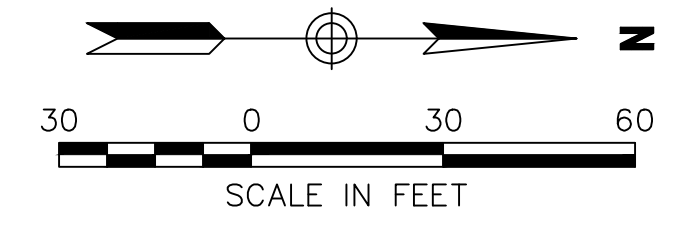
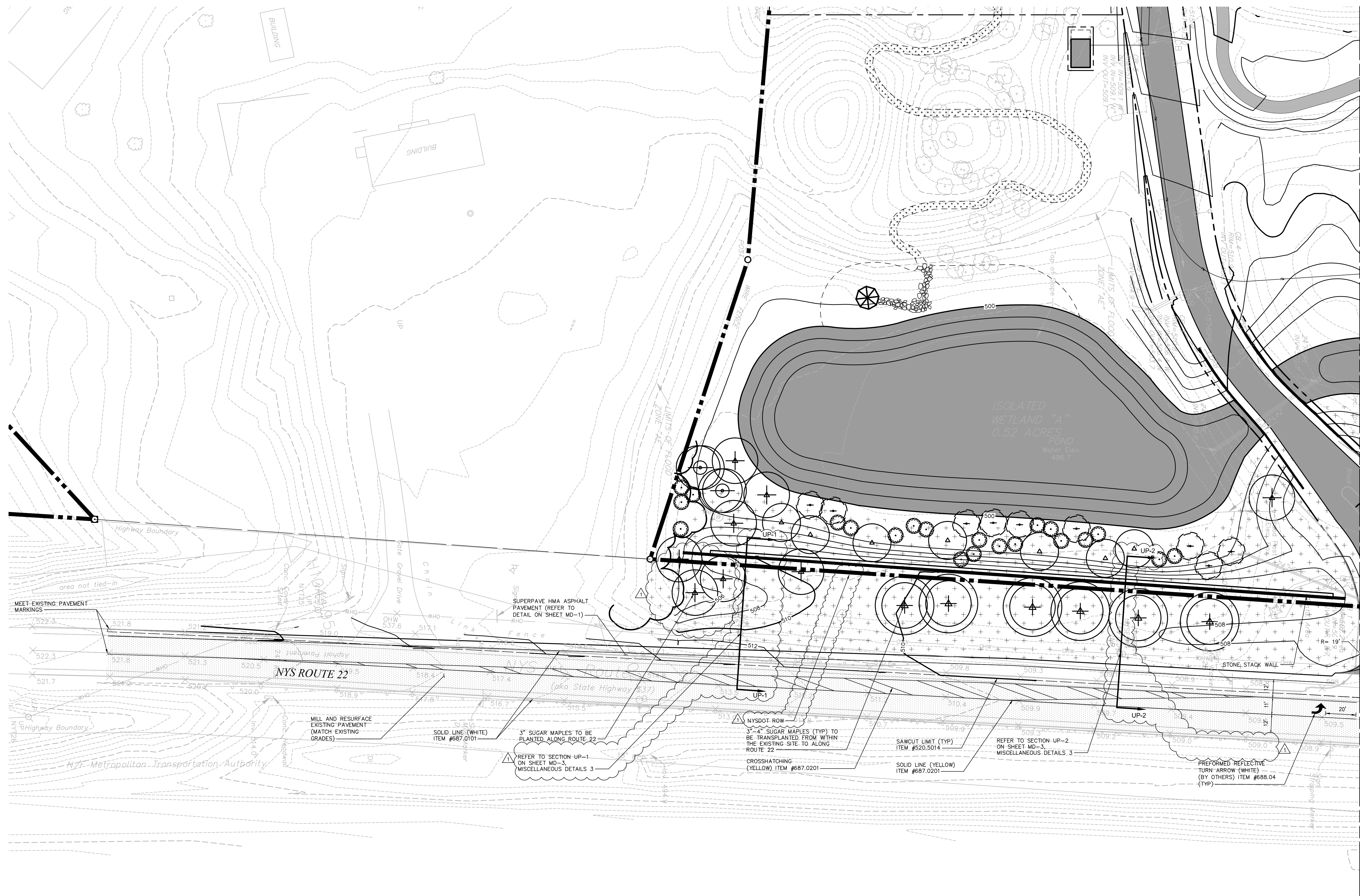
Sheet **18**

Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690

Project Number
29011.00



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Scale	As Shown	Date	July 31, 2014		
Project Title					

**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York
Issued for
NYS DOT Permit

Not Approved for Construction
Drawing Title
General Plan 1

MAIN ENTRANCE - 1

- Notes:**
- FOR WORK ZONE TRAFFIC CONTROL REFER TO WORK ZONE TRAFFIC CONTROL PLAN 1 AND 2
 - FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SD-1, SIGHT DISTANCE TRIANGLES 1.
 - FOR CROSS SECTIONS UP-1 AND UP-2 REFER TO MD-3, MISCELLANEOUS DETAILS 3.

Drawing Number

GP-1

Sheet 3 of 18

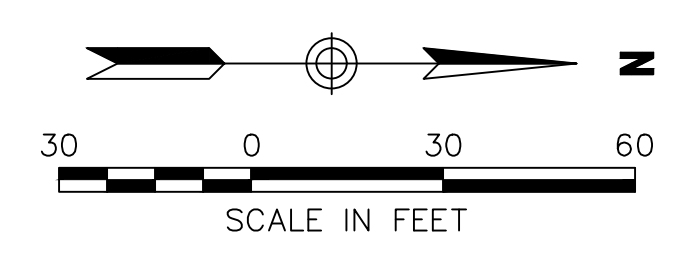
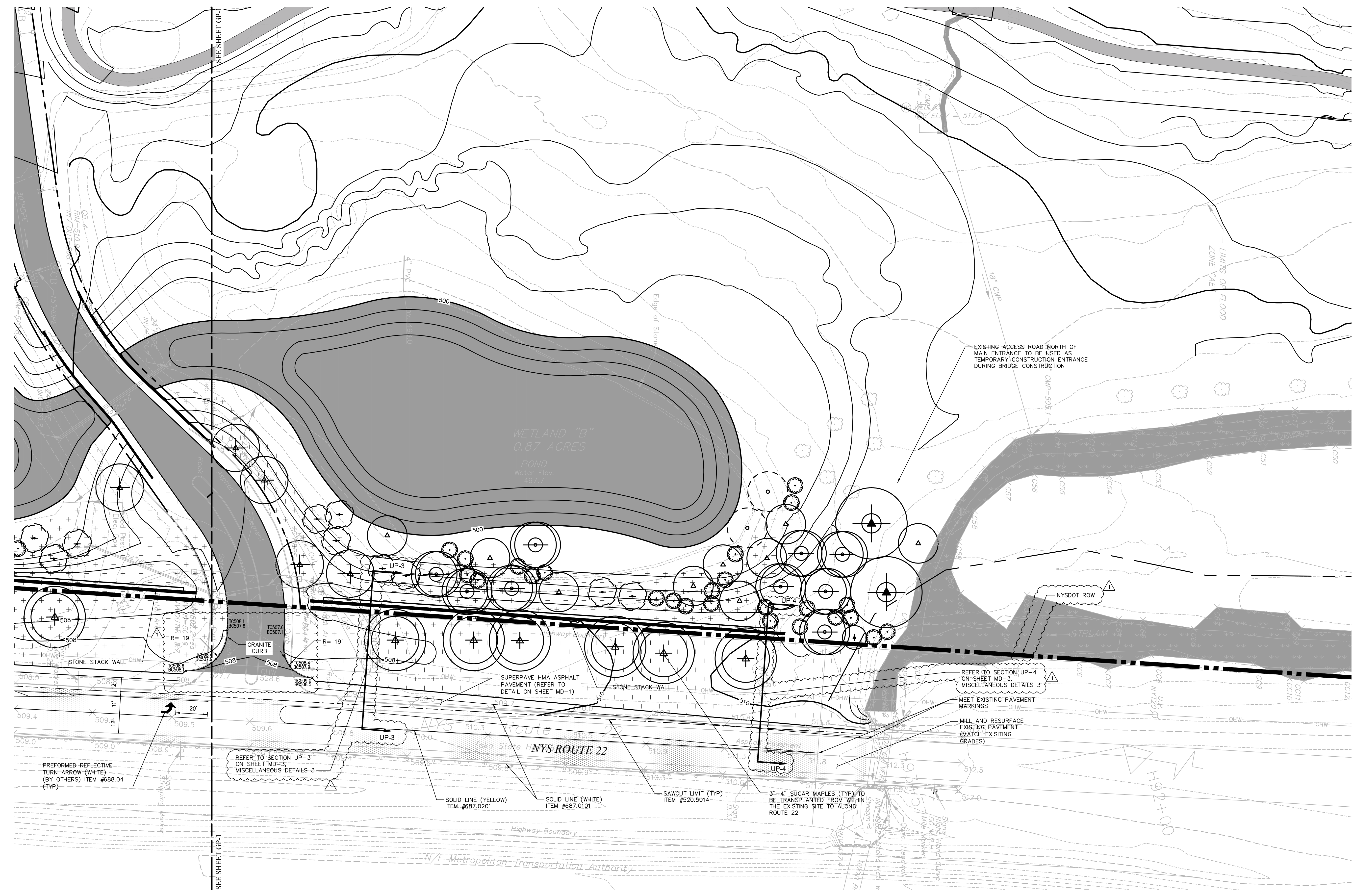
Amanda C. DeCesare
NYS Professional Engineer
NY Lic. No. 084690

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29011.00

29011.00-NYS DOT-LOT.DWG



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 4651 Route 22
 Amenia, New York
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 Drawing Title
General Plan 2

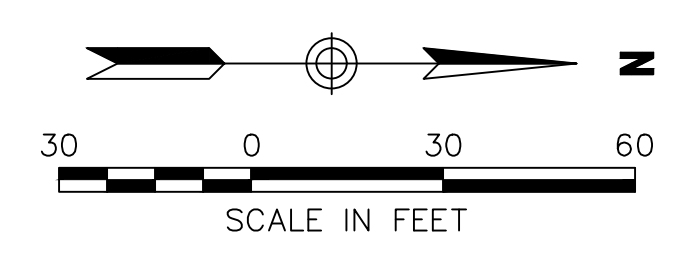
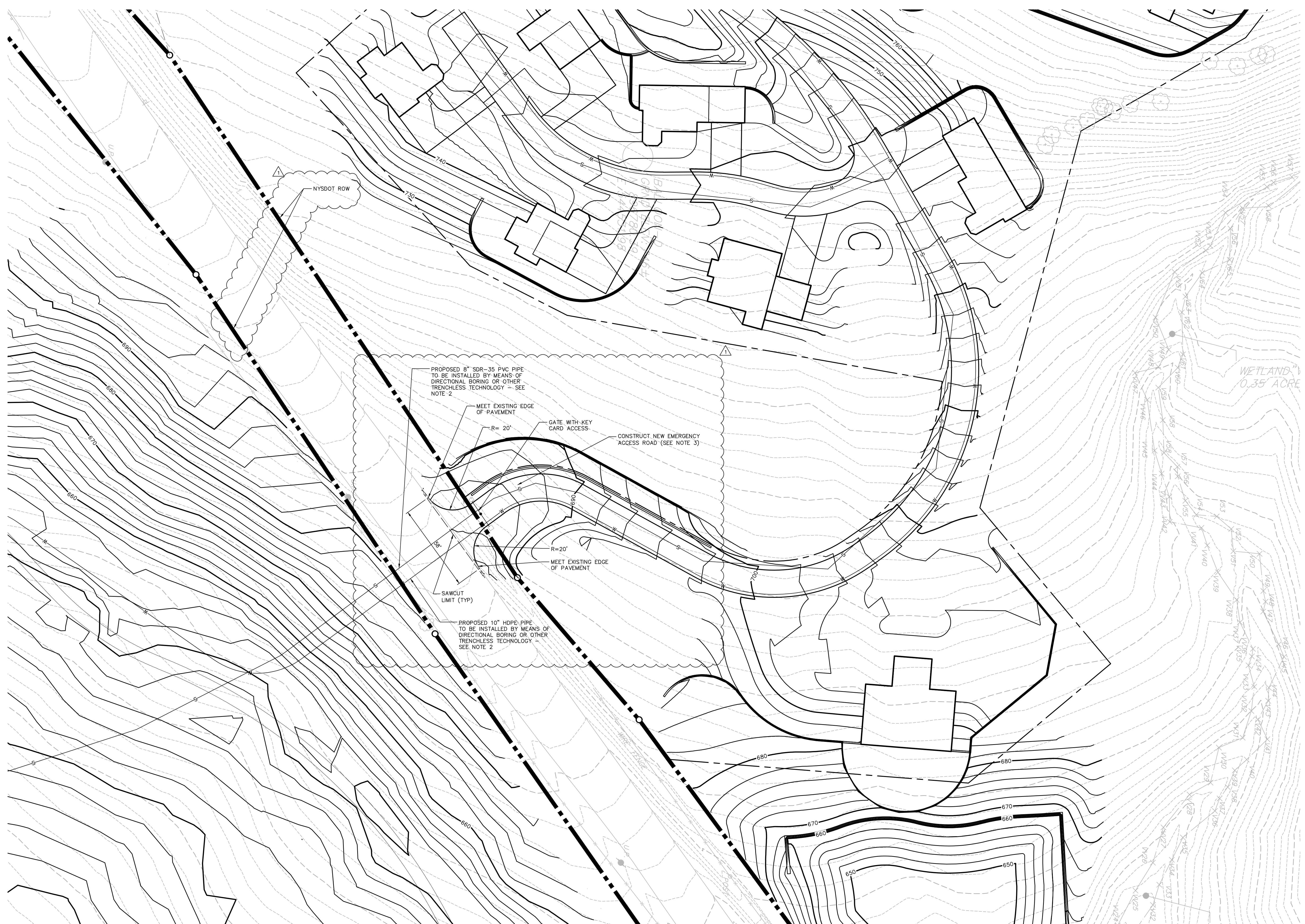
MAIN ENTRANCE - 2

- Notes:**
1. FOR WORK ZONE TRAFFIC CONTROL REFER TO WORK ZONE TRAFFIC CONTROL PLAN 1 AND 2.
 2. FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SD-1, SIGHT DISTANCE TRIANGLES 1.
 3. FOR CROSS SECTIONS UP-3 AND UP-4 REFER TO MD-3, MISCELLANEOUS DETAILS 3.

Drawing Number
GP-2
 Sheet 18 of 18
 Project Number
 29011.00
 Amanda C. DeCesare
 N.Y. Professional Engineer
 NY Lic. No. 084690



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Drawing Title
General Plan 3

Drawing Number

VINEYARD COTTAGES ENTRANCE

Notes:

1. FOR WORK ZONE TRAFFIC CONTROL REFER TO WORK ZONE TRAFFIC CONTROL PLAN 3.
2. A USE AND OCCUPANCY PERMIT IS REQUIRED FOR THE UTILITIES WITHIN THE NYS DOT RIGHT OF WAY (REFER TO DETAIL ON SHEET MD-2).
3. THE ENTRANCE SHALL BE GATED FOR EMERGENCY ACCESS ONLY.
4. FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SD-2, SIGHT DISTANCE TRIANGLES 2.

Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690

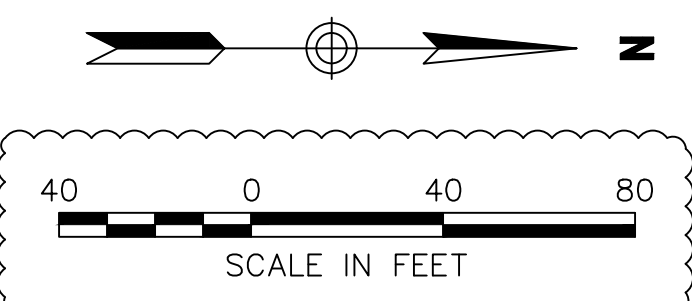
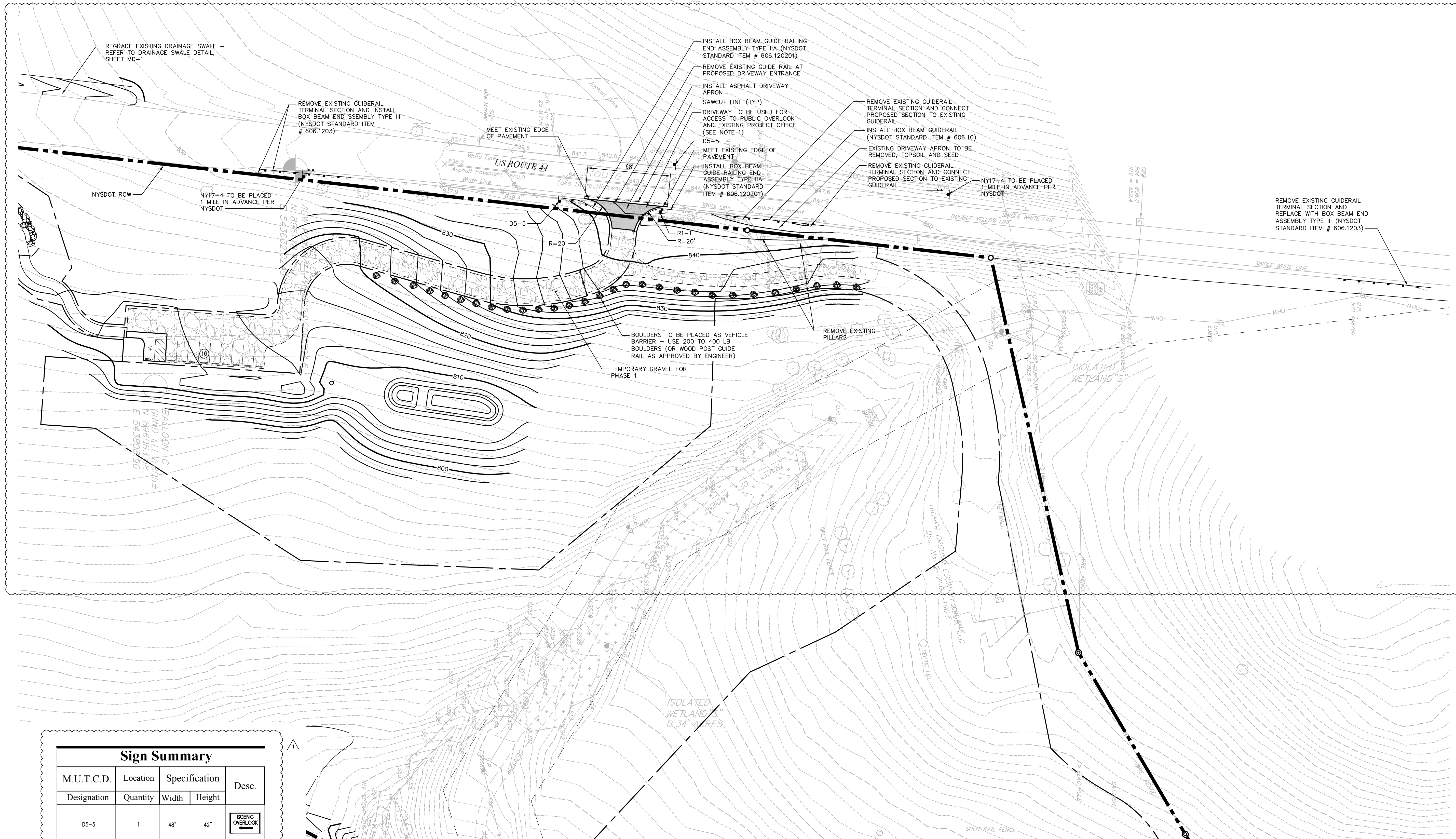
GP-3
Sheet 18 of 18
Project Number
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**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York

Issued for
NYS DOT Permit

Construction
Drawing Title

**General Plan 4
Phase 1**

M.U.T.C.D.	Location	Specification	Desc.	
Designation	Quantity	Width	Height	
D5-5	1	48"	42"	
D5-5	1	48"	42"	
NY17-4	2	60"	30"	

WINERY ENTRANCE

Notes:

- DETAILED PLANS FOR ACCESS TO FUTURE WINERY AND VINEYARD COTTAGES TO BE PROVIDED DURING SITE PLAN PHASE 2.
- FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SHEET SD-2, SIGHT DISTANCE TRIANGLES 2.

Drawing Number

GP-4A

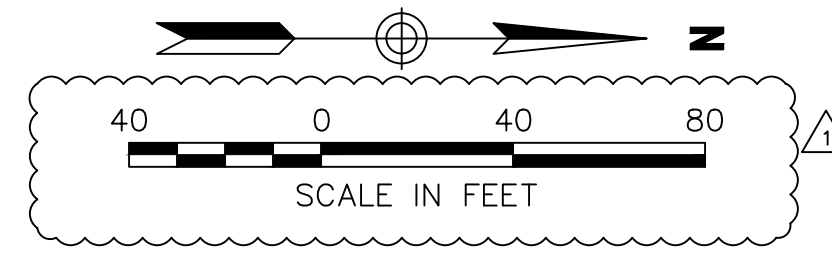
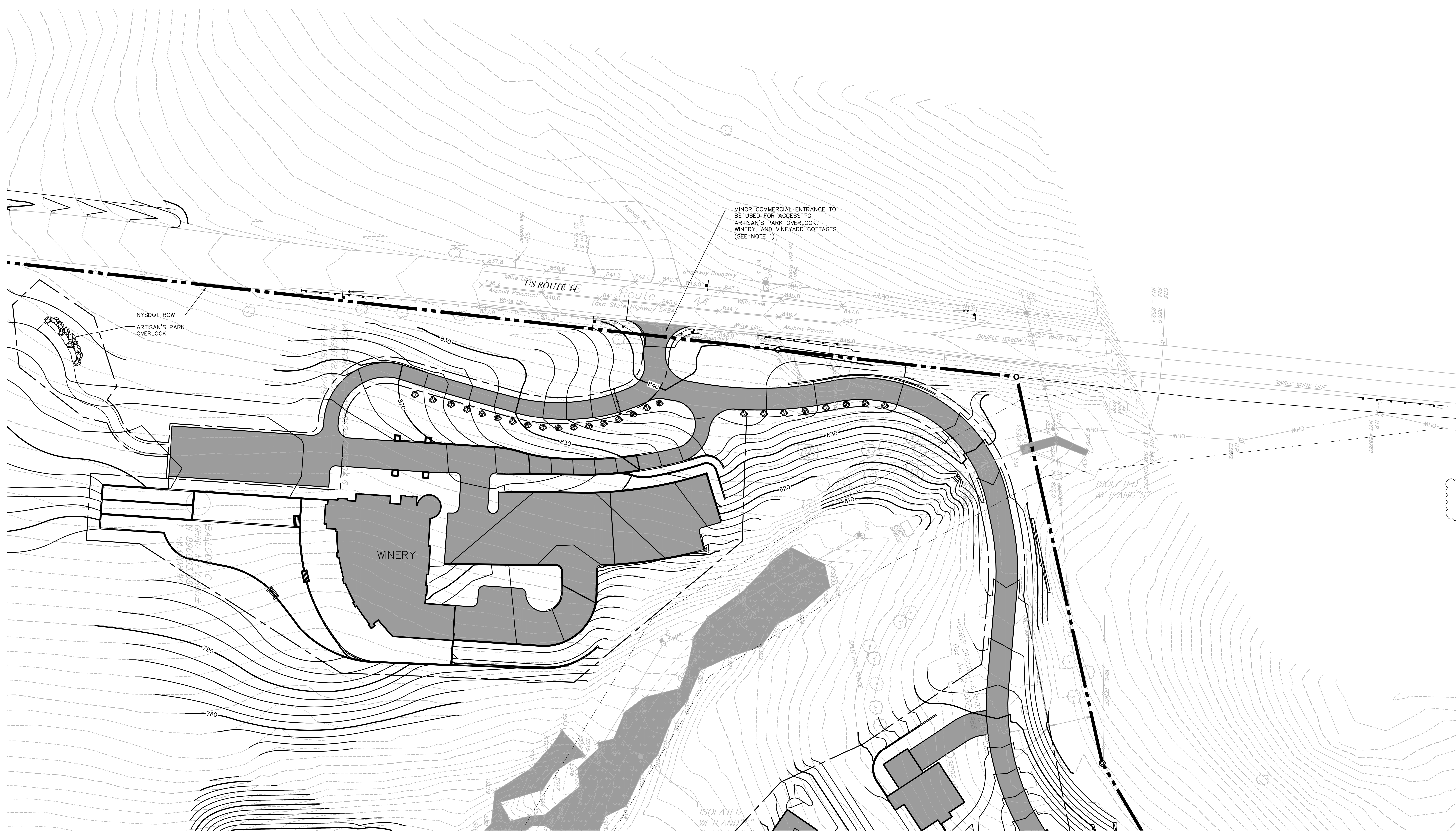
Sheet 6 of 18

Project Number
29011.00

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NY Lic. No. 084690



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 4651 Route 22
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**General Plan 4
MDP**

Drawing Number

GP-4B

Sheet 18

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 NY Lic. No. 084690

Project Number
 29011.00

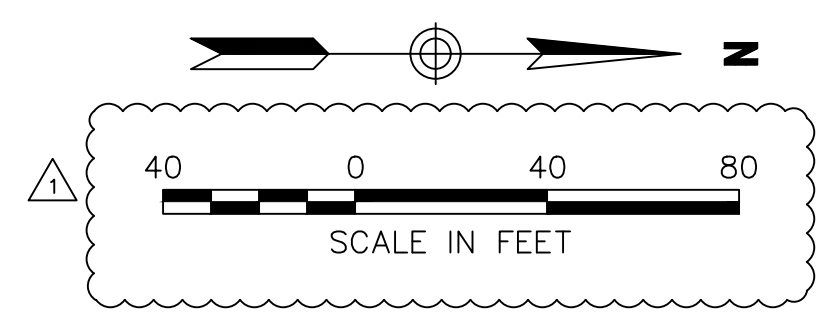
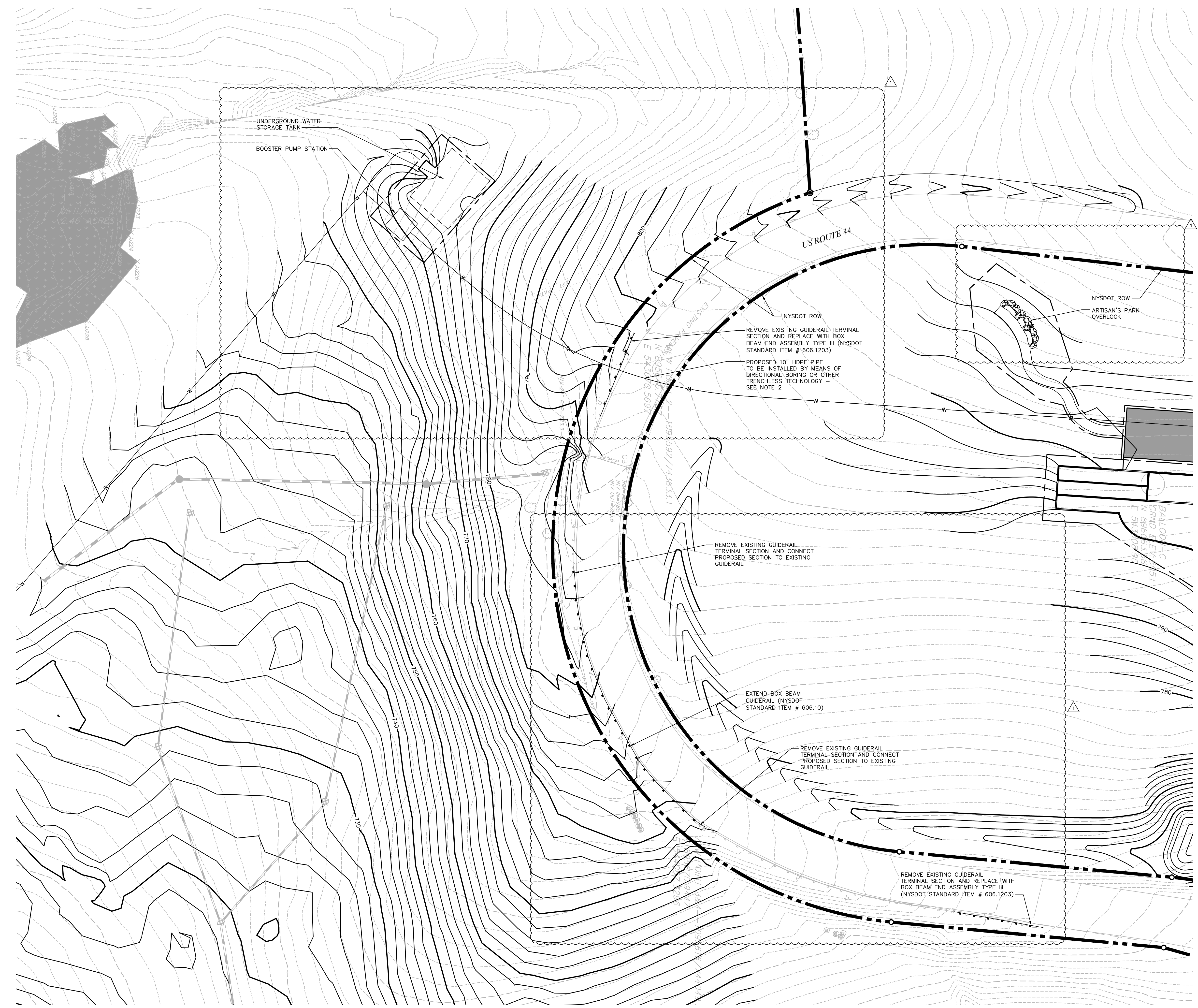
WINERY ENTRANCE

- Notes:**
- ACCESS TO OVERLOOK TO BE BUILT DURING PHASE 1 AS SHOWN. DETAILED PLANS FOR ACCESS TO FUTURE WINERY AND VINEYARD COTTAGES TO BE PROVIDED DURING SITE PLAN PHASE 2.
 - FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SHEET SD-2, SIGHT DISTANCE TRIANGLES 2.

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CAD checked by	MB	Approved by	ACD	Scale	As Shown	Date	July 31, 2014

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**General Plan 5
Option 1**

Drawing Number

GP-5A

Sheet 8 of 18

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NY Lic. No. 084690

Project Number
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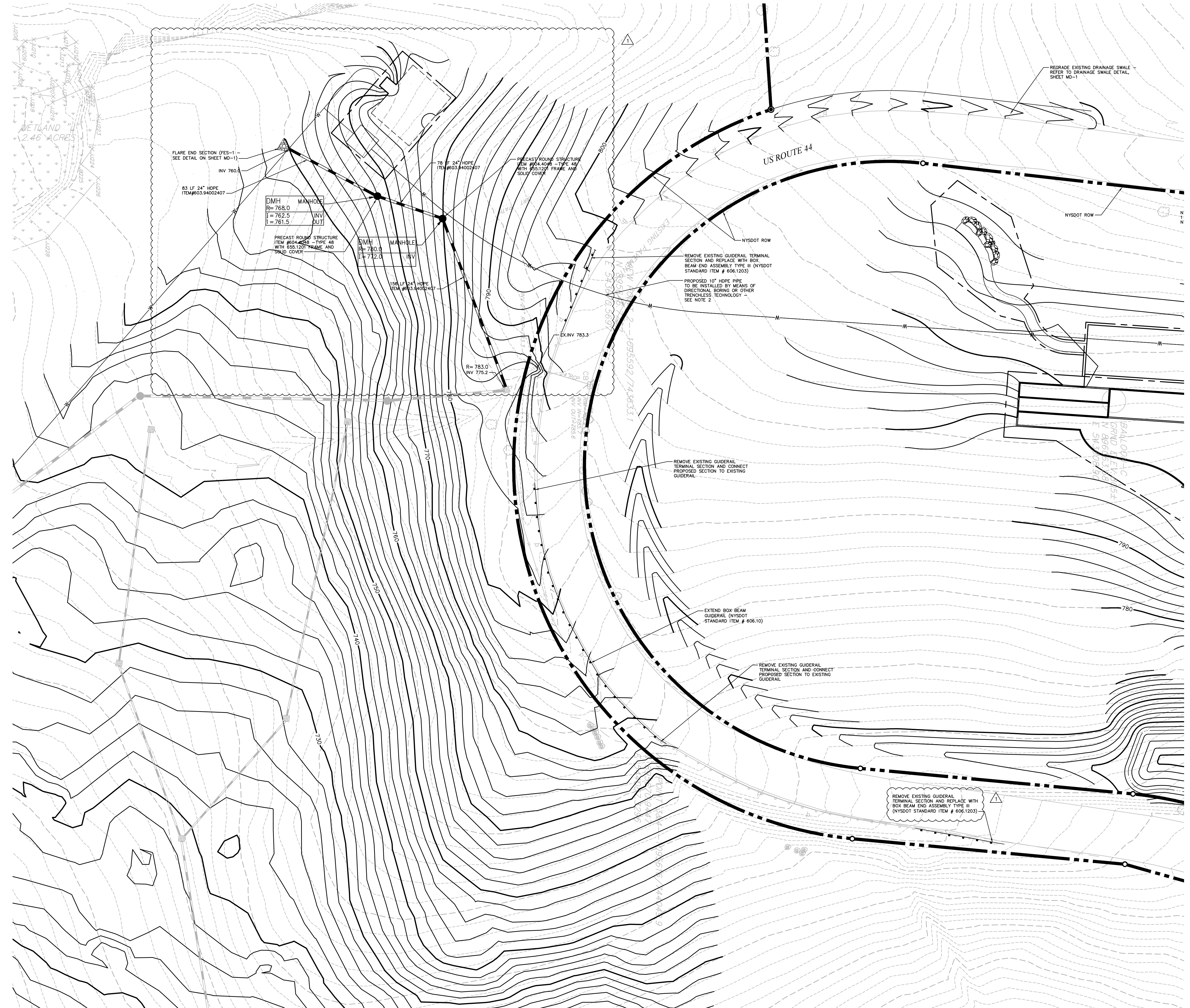
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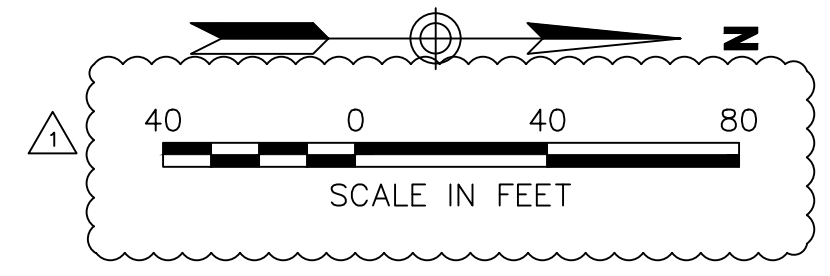
- 1. A USE AND OCCUPANCY PERMIT IS REQUIRED FOR THE UTILITIES WITHIN THE NYSDOT RIGHT OF WAY (REFER TO DETAIL ON SHEET MD-2).

Saved Tuesday, February 17, 2015 9:54:12 AM DROSENBERG Plotted Tuesday, February 17, 2015 10:08:22 AM Roseberg, David

\\VHB\PROJ\WHITEPLAINS\29011.00 APWAN\CAD\LD\PLANSET\NYS DOT PERMIT PLANS\29011.00-NYS DOT-PLOT



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No.	Revision	Date	Appvd.
Designed by	ACD	Drawn by	DAR
CAD checked by	MB	Approved by	ACD
Scale	As Shown	Date	July 31, 2014

**Silo Ridge
Resort Community**

4651 Route 22
 Amenia, New York
 Issued for
 NYS DOT Permit

Not Approved for Construction
 Drawing Title
**General Plan 5
Option 2**

Drawing Number
GP-5B
 Sheet 9 of 18
 Project Number
 29011.00

HAIRPIN TURN

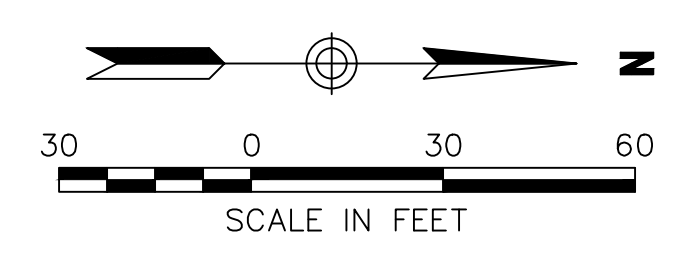
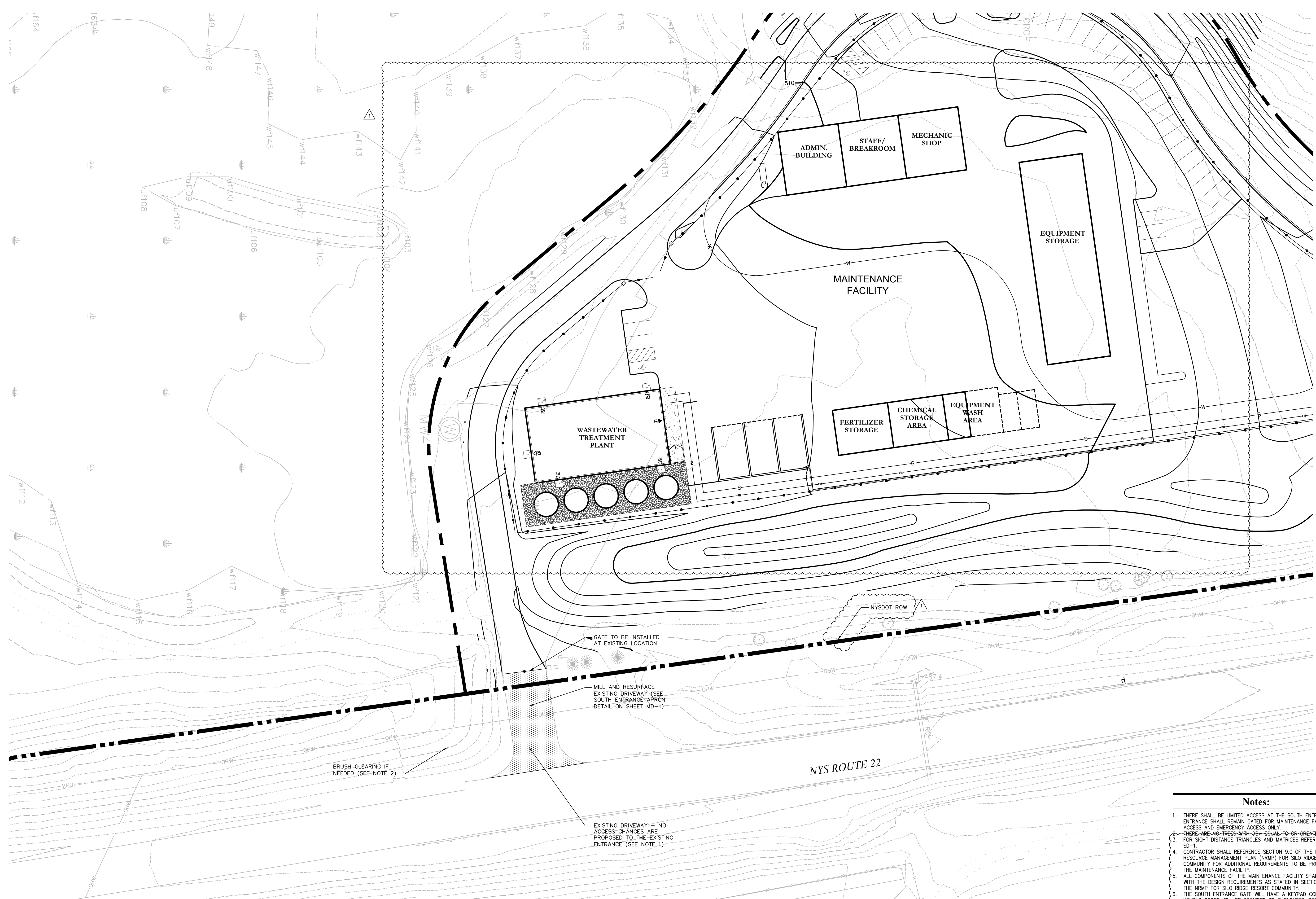
Notes:
 1. A USE AND OCCUPANCY PERMIT IS REQUIRED FOR THE UTILITIES WITHIN THE NYS DOT RIGHT OF WAY (REFER TO DETAIL ON SHEET MD-2).

Amanda C. DeCesare
 N.Y. Professional Engineer
 NY Lic. No. 084690

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CAD checked by	MB	Approved by	ACD	Scale	As Shown	Date	July 31, 2014

Silo Ridge Resort Community

4651 Route 22
Amenia, New York
Issued for
NYS DOT Permit

Not Approved for Construction
Drawing Title
General Plan 6

Drawing Number
GP-6
Sheet 10 of 18
Project Number
29011.00
Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690

- Notes:**
1. THERE SHALL BE LIMITED ACCESS AT THE SOUTH ENTRANCE. THE ENTRANCE SHALL REMAIN GATED FOR MAINTENANCE FACILITY ACCESS AND EMERGENCY ACCESS ONLY.
 2. THERE ARE NO TREES WITH DBH EQUAL TO OR GREATER THAN 6".
 3. FOR SIGHT DISTANCE TRIANGLES AND MATRICES REFER TO SHEET SD-1.
 4. CONTRACTOR SHALL REFERENCE SECTION 9.0 OF THE NATURAL RESOURCE MANAGEMENT PLAN (NRMP) FOR SILO RIDGE RESORT COMMUNITY FOR ADDITIONAL REQUIREMENTS TO BE PROVIDED AT THE MAINTENANCE FACILITY.
 5. ALL COMPONENTS OF THE MAINTENANCE FACILITY SHALL COMPLY WITH THE DESIGN REQUIREMENTS AS STATED IN SECTION 9.0 OF THE NRMP FOR SILO RIDGE RESORT COMMUNITY.
 6. THE SOUTH ENTRANCE GATE WILL HAVE A KEYPAD CONTROL. KEYPAD CODES WILL BE PROVIDED TO EMPLOYEES, RESIDENTS, AND EMERGENCY SERVICES. THE GOLF MAINTENANCE FACILITY AND THE MAIN ENTRY GATEHOUSE WILL ALSO HAVE THE ABILITY TO CONTROL THE GATE. THE GATE MAY REMAIN OPEN DURING PEAK INGRESS AND EGRESS.
 7. A GATE ARM WILL BE PROVIDED JUST BEYOND THE ENTRANCE TO THE GOLF MAINTENANCE FACILITY. EMPLOYEES, RESIDENTS, AND EMERGENCY SERVICES WILL HAVE A KEY PASS WHICH WILL PROVIDE ACCESS THROUGH THE GATE ARM. THE GATE ARM CAN ALSO BE CONTROLLED BY THE GOLF MAINTENANCE FACILITY.

SOUTH ENTRANCE

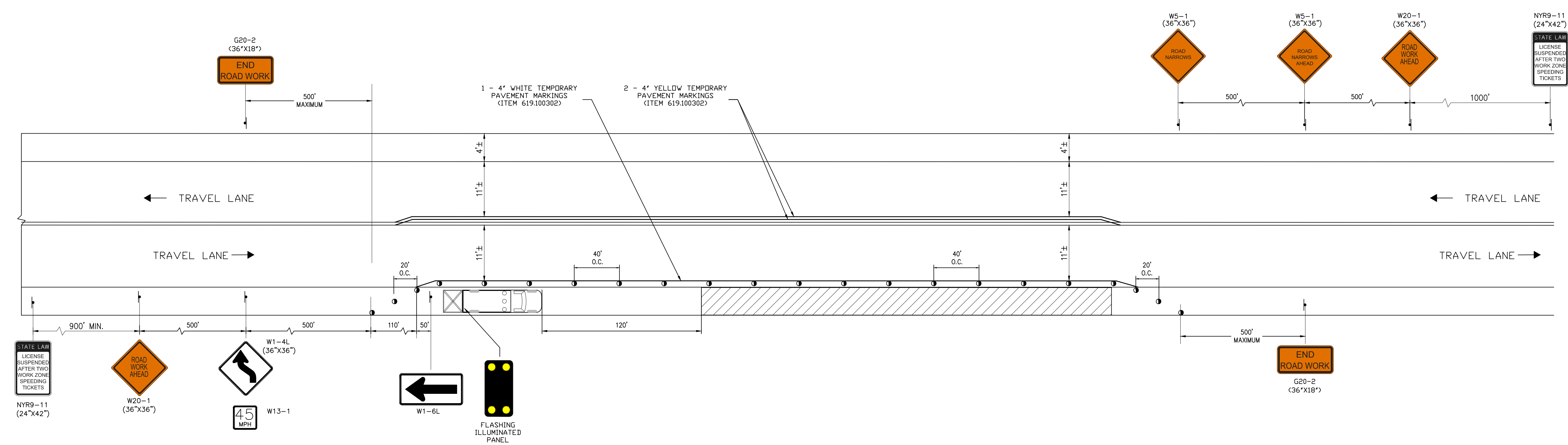
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**Work Zone Traffic Control Plan - Lane Shift
For Proposed Road Widening**
N.T.S.

- Notes:**
1. COVER OR REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND SIGNS PER ITEM 619.080202 OR 619.0803.
 2. SIGNS ARE ALL SIZED FOR A CONVENTIONAL ROAD PER NYS DOT STANDARD SHEET 619-12.

LEGEND

N.T.S.

- FLASHING ILLUMINATED PANEL
- DRUM
- WORK AREA
- DIRECTION OF TRAFFIC

No.	Revision	Date	Appvd.
AS PER NYS DOT COMMENTS		1/8/2015	ACD
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Checked by	MWJ	Approved by	ACD
CAD checked by	MB	Date	July 31, 2014
Scale	N.T.S.	Project Title	

**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York
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Drawing Title

**Work Zone
Traffic Control
Plan 1**

Drawing Number

WZ-1

Sheet 11 of 18

Project Number
29011.00

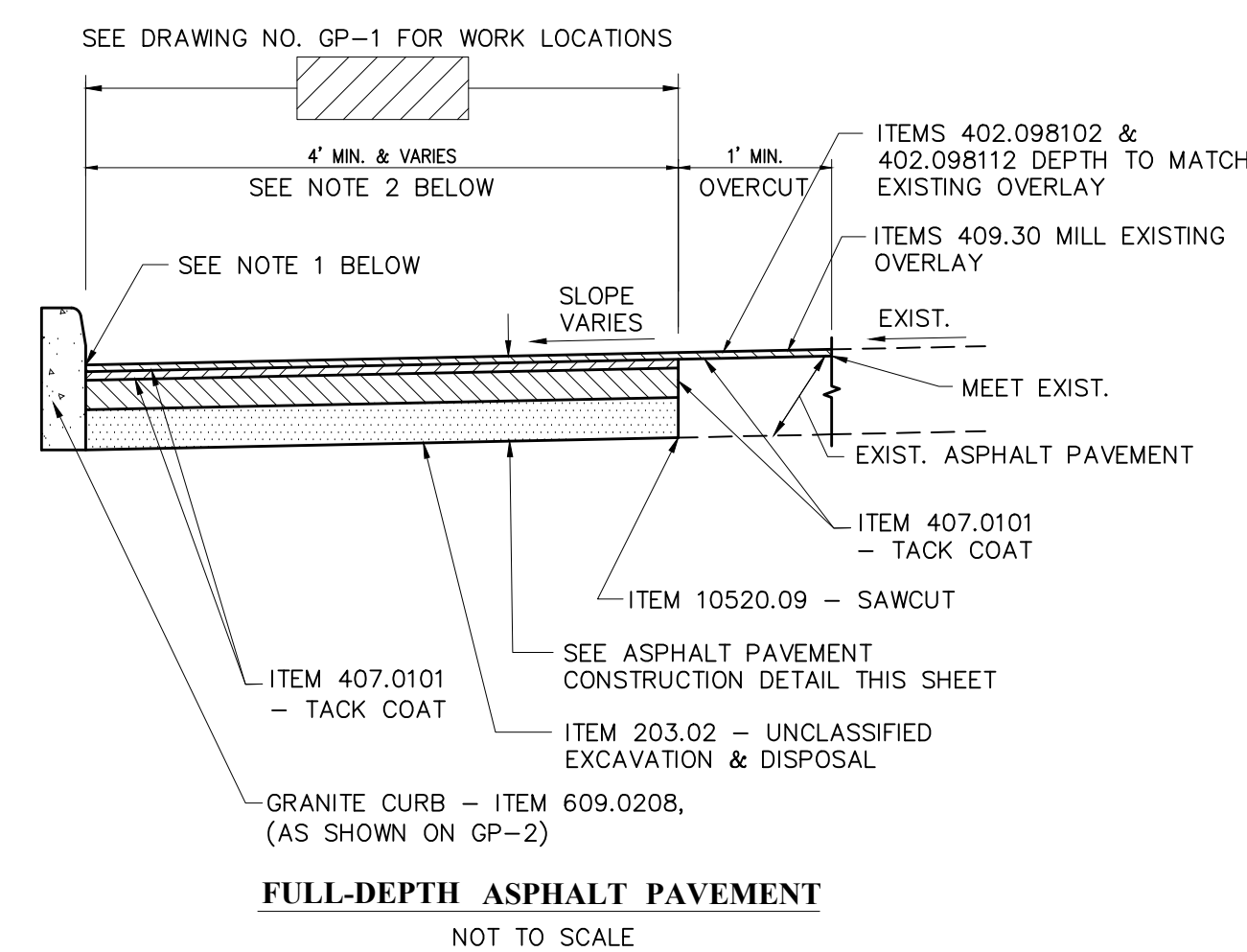
Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690



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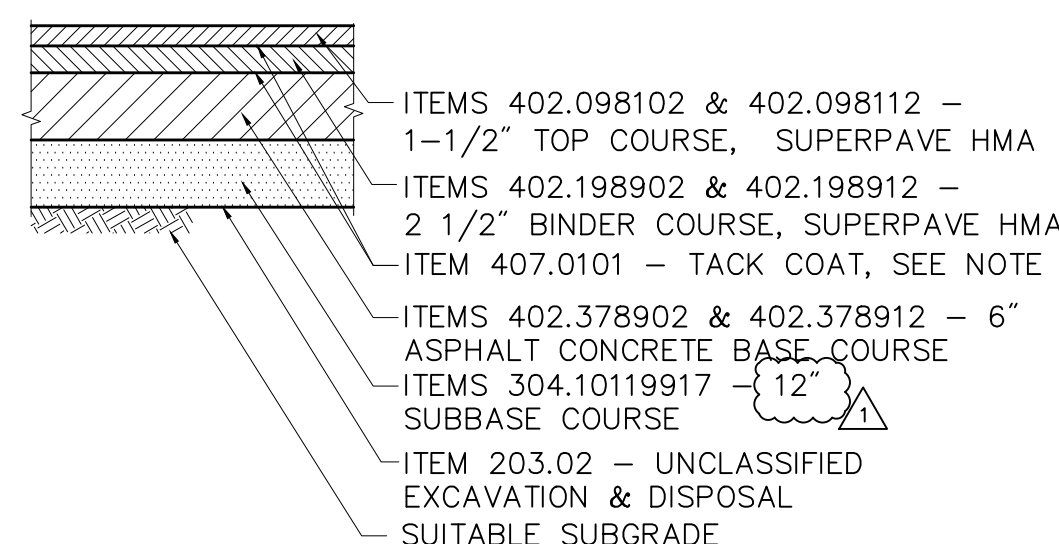
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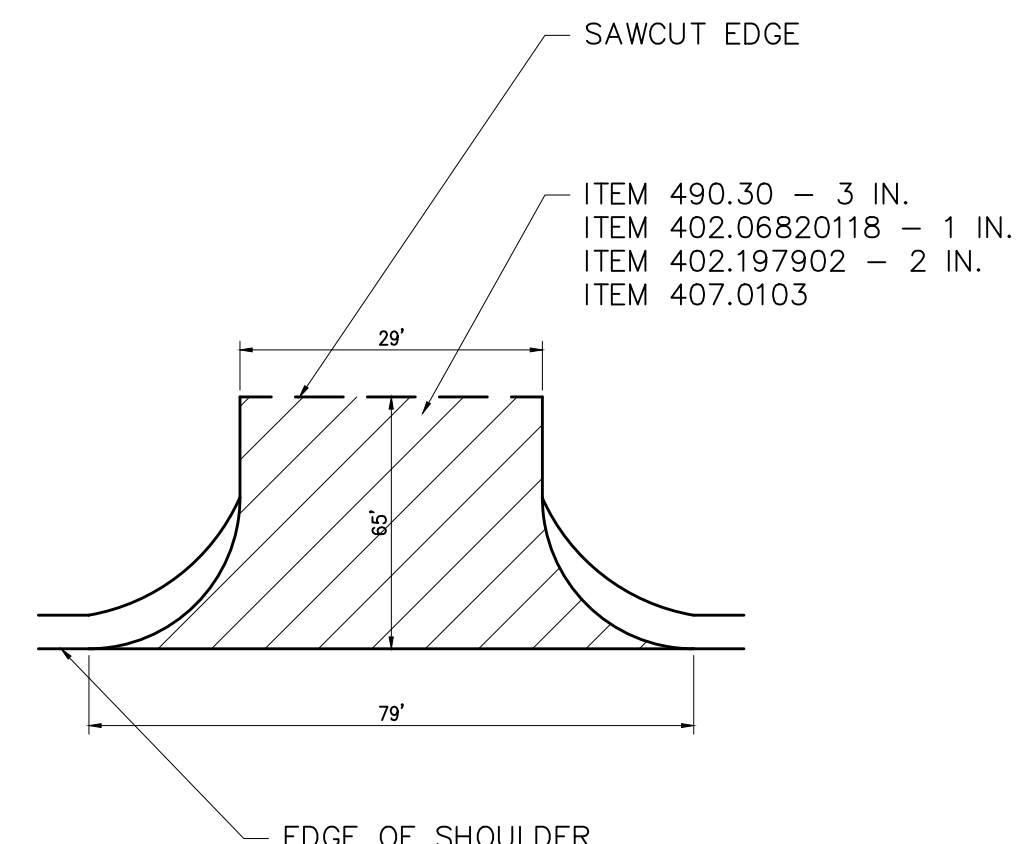
Asphalt Pavement Reconstruction Detail

PAVEMENT RESTORATION NOTES:
1. REFER TO DRAWING NUMBERS GP-1 FOR ALL PROPOSED TOP AND BOTTOM OF CURB GRADES.
2. VARY DEPTH OF ASPHALT BASE TO ENSURE PROPOSED PAVEMENT DEPTH MATCHES ADJACENT EXISTING PAVEMENT DEPTH.
3. ANY EXISTING FULL DEPTH ASPHALT PAVEMENT ENCOUNTERED SHALL BE RECONSTRUCTED WITH FULL STRENGTH ASPHALT PAVEMENT, SEE ASPHALT PAVEMENT CONSTRUCTION DETAIL.



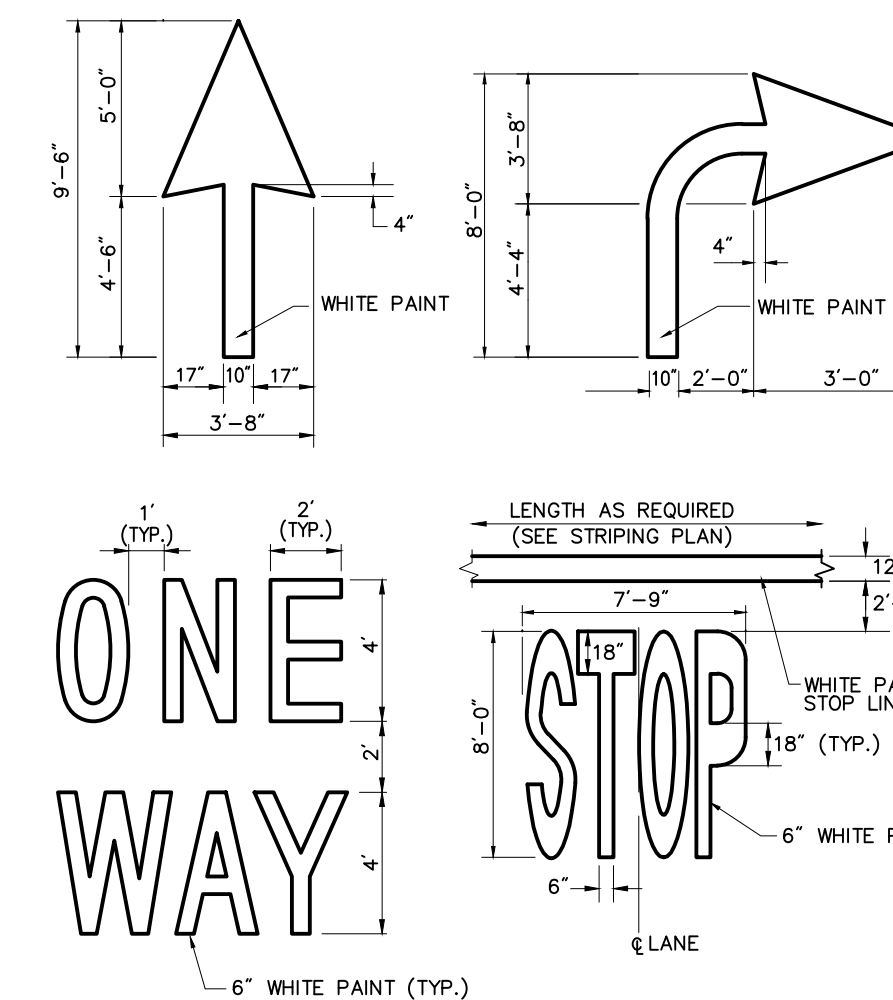
Superpave HMA Asphalt Pavement Detail

N.T.S.
NOTE:
PLACE TACK COAT BETWEEN EVERY ASPHALT LIFT AND AT THE JOINTS BETWEEN NEW AND EXISTING ASPHALT PAVEMENT EDGES.



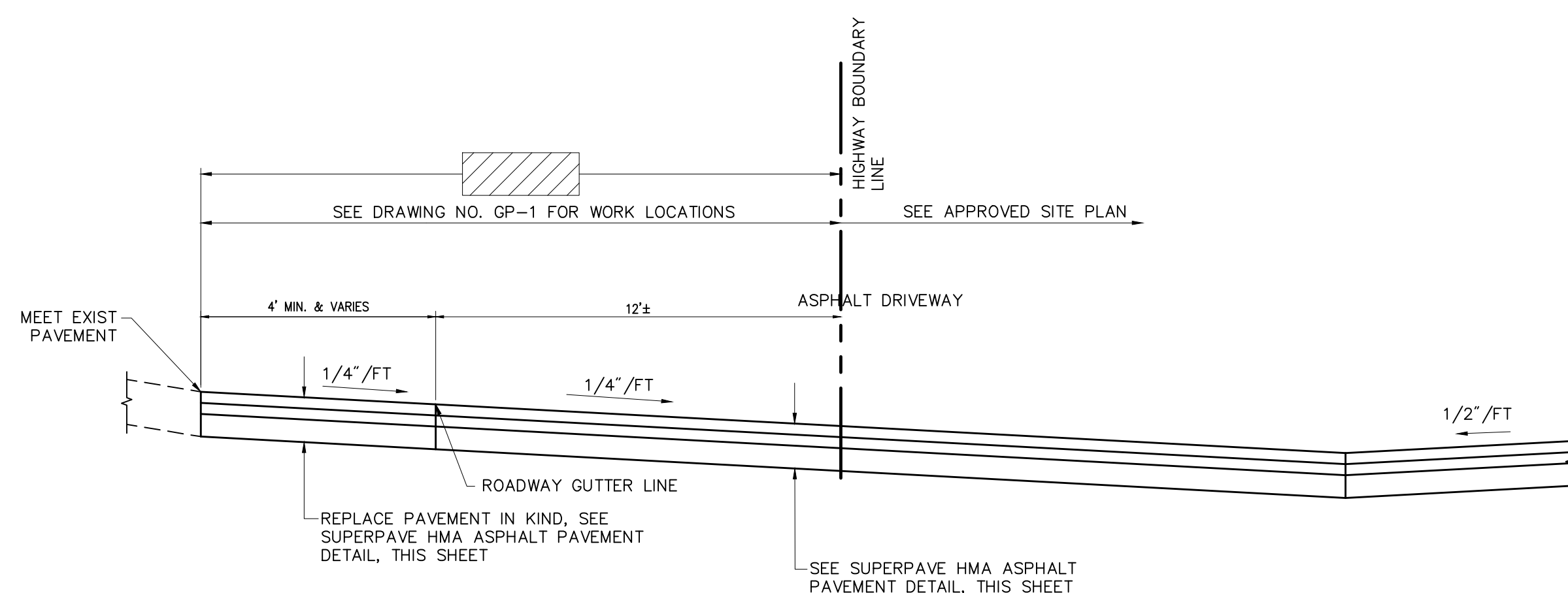
South Entrance Apron

N.T.S. Source: NYS DOT



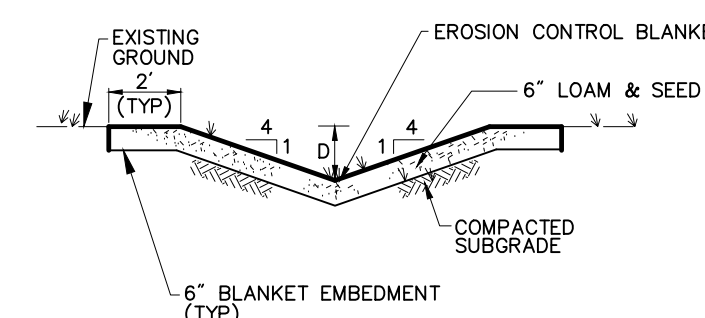
Painted Pavement Markings - Highway

N.T.S. Source: VHB 1/12 LD_555



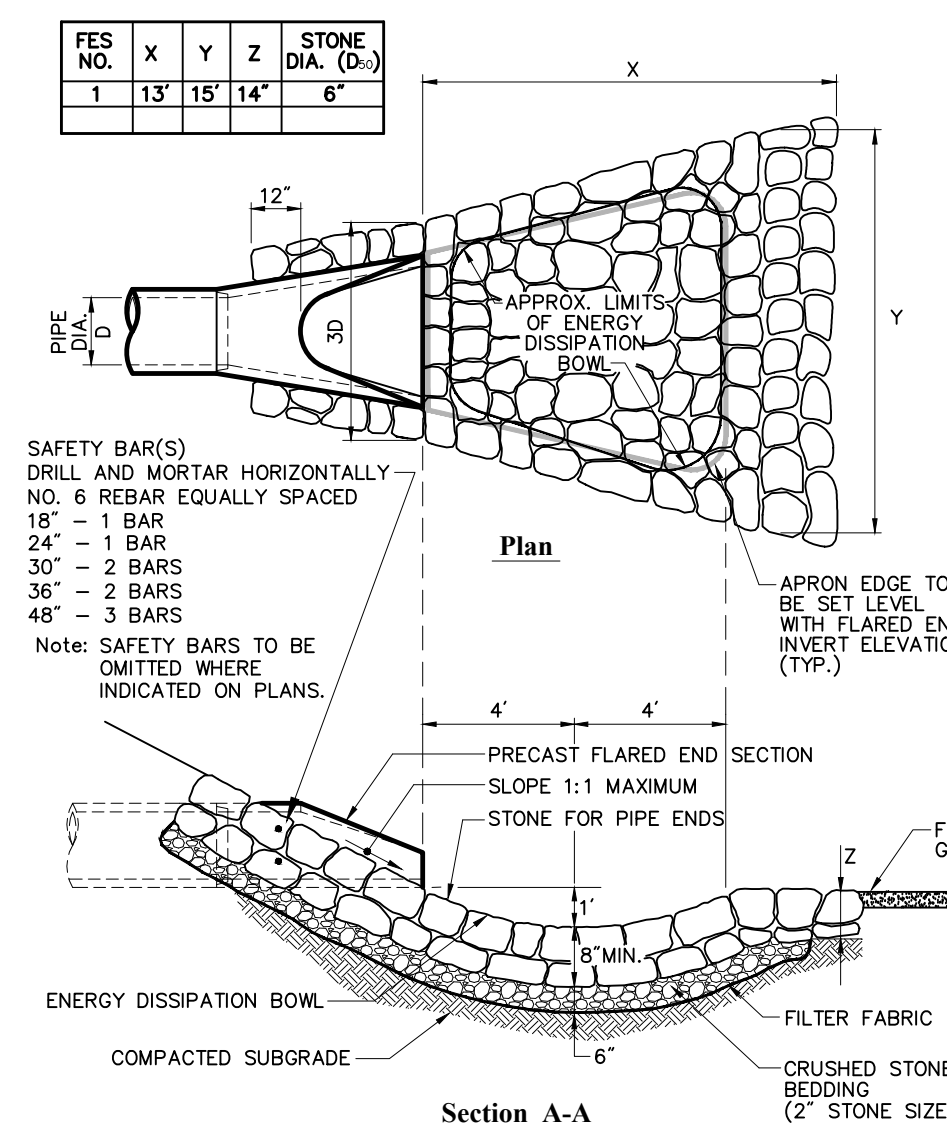
Typical Driveway Detail Section

N.T.S.



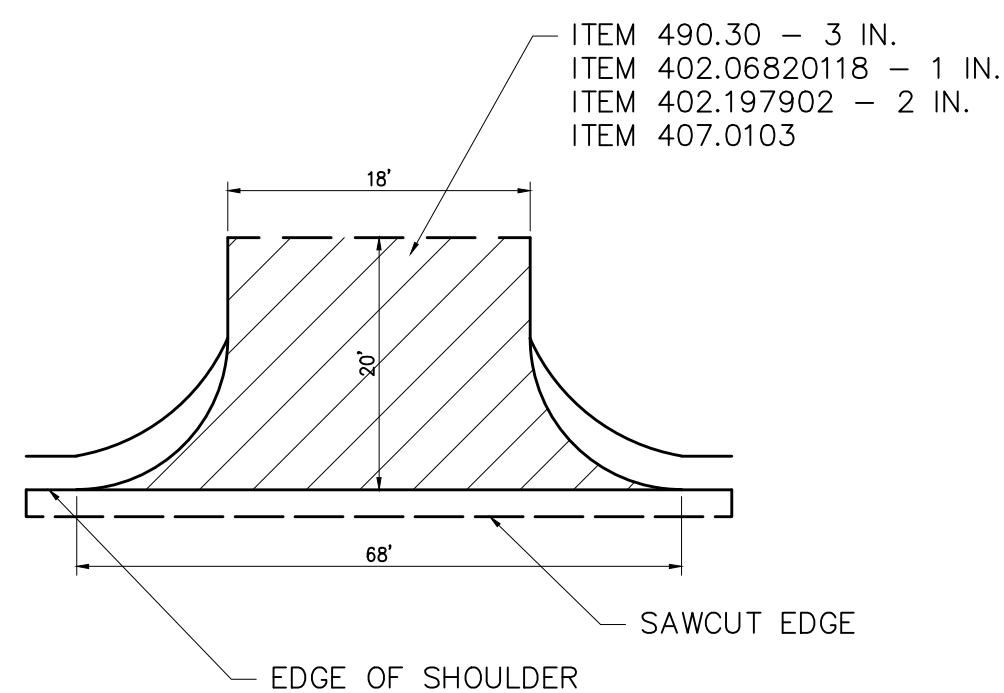
Drainage Swale

N.T.S. Source: VHB 6/08 LD_171



Flared End Section (FES) with Stone Protection

N.T.S. Source: VHB 5/12 LD_134



Overlook Entrance Apron

N.T.S. Source: NYS DOT

AS PER NYS DOT COMMENTS	1/8/2015	ACD
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Project Title		

**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York
Issued for
NYS DOT Permit

Not Approved for Construction

Miscellaneous Details 1

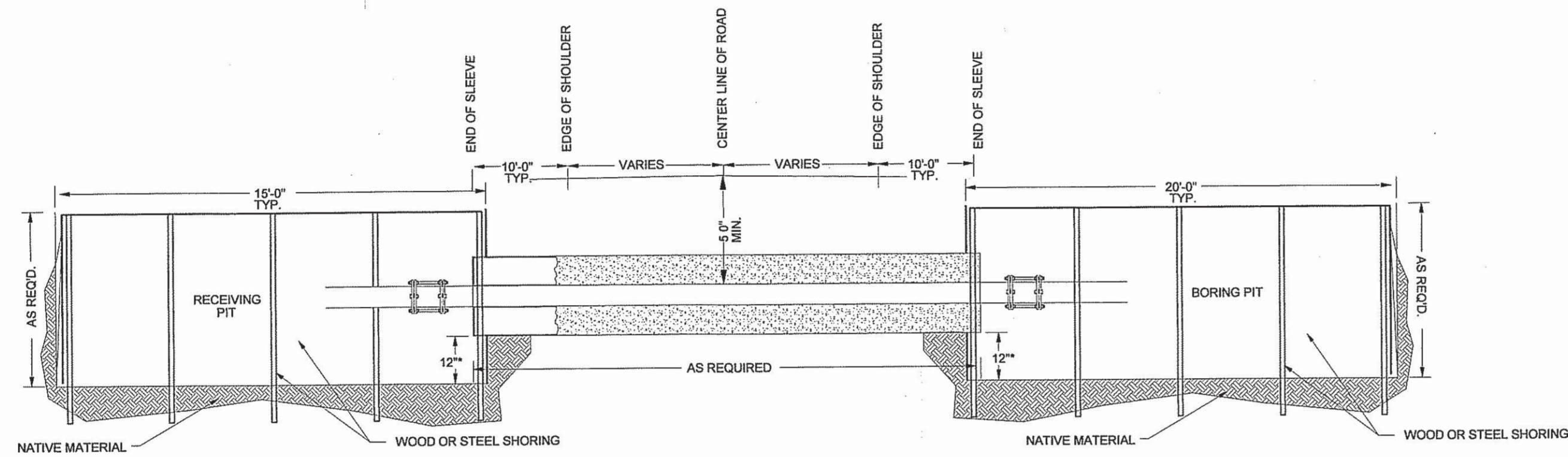
Drawing Number

MD-1

Sheet 14 of 18

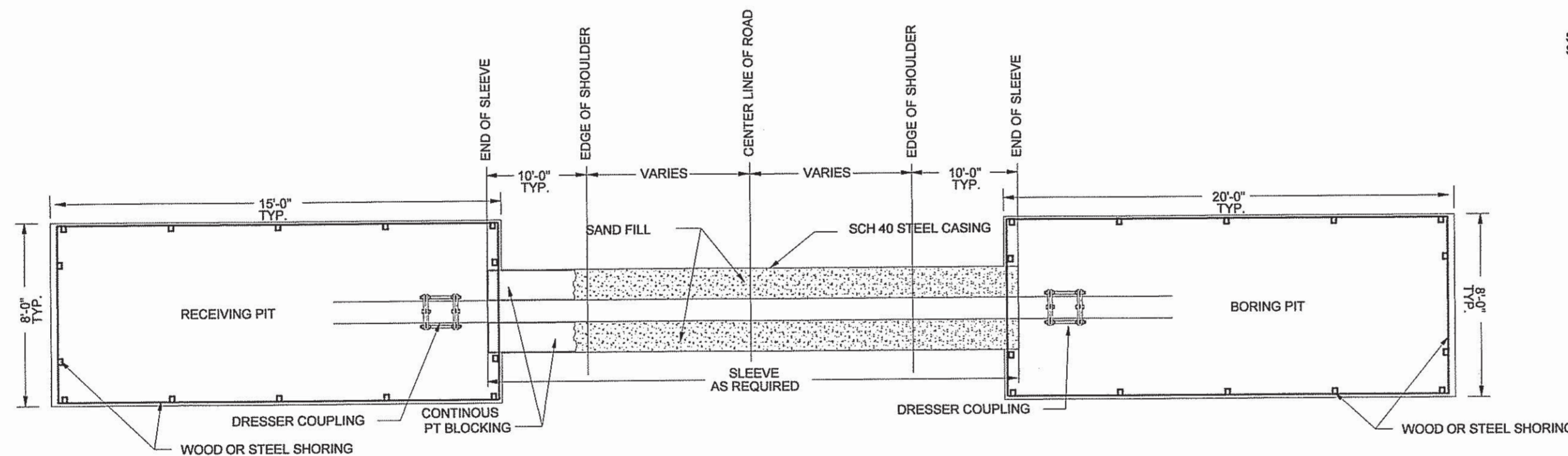
Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690

Project Number
29011.00



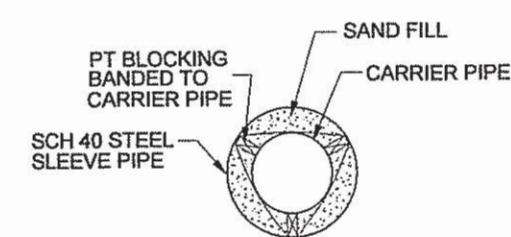
TYPICAL BORING SECTION

NTS



TYPICAL BORING PLAN


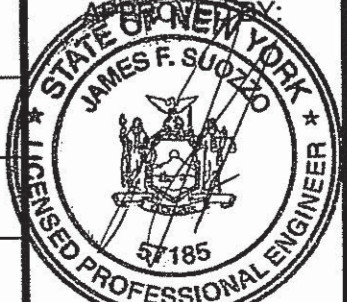
NTS



STEEL CASING DETAIL

ROAD BORING TABLE	
PIPE SERVICE	SLEEVE SIZE
8" WATER MAIN	14" SLEEVE
8" GRAVITY SEWER	14" SLEEVE
8" FORCEMAIN (OUTFALL)	14" SLEEVE
10" GRAVITY SEWER	16" SLEEVE

NOTE: ALL PIPES SHALL BE INSTALLED IN SCH 40 STEEL CASING, PRESSURE TREATED BLOCKING SHALL BE ADJUSTED FOR SPECIFIED PIPE DIAMETER
 NYS DOT HIGHWAY WORK PERMIT AND INSURANCE AS REQUIRED TO BE SUPPLIED BY OTHERS.
 CONTRACTOR SHALL PROVIDE DETAILED SHORING DRAWINGS, ALL DRAWINGS SHALL BE STAMPED BY A NYS LICENSED ENGINEER

REVISIONS		DRAWING TITLE:	
NO.	DATE	DESCRIPTION	
			GENERAL SITE DETAILS
UNAUTHORIZED ALTERATION OR ADDITION TO THIS DOCUMENT IS A VIOLATION OF SECTION 7209 SUBDIVISION 2 OF THE NEW YORK STATE EDUCATION LAW.			CLIENT:
 CEDARWOOD ENGINEERING SERVICES, PLLC DELAWARE OPERATIONS, INC. CIVIL & ENVIRONMENTAL ENGINEERING AND FACILITY OPERATIONS 8-12 DIETZ STREET, SUITE 303, ONEONTA, NY 607.432.8073 248 MAIN STREET, PO BOX 203, NORTH CREEK, NY 818.281.5160			SCALE: AS SHOWN DATE: 8-14 FILENAME:
DESIGNED BY:	JFS		GRAPHIC SCALE
DRAWN BY:	DJO		0
REVIEWED BY:	JEP		SCALE: AS SHOWN
CHECKED BY:	JFS		DATE: 8-14
SHEET NO.			MD-2

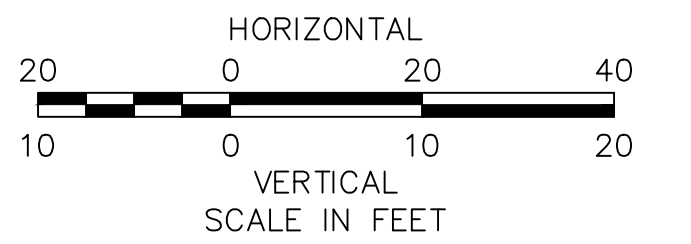
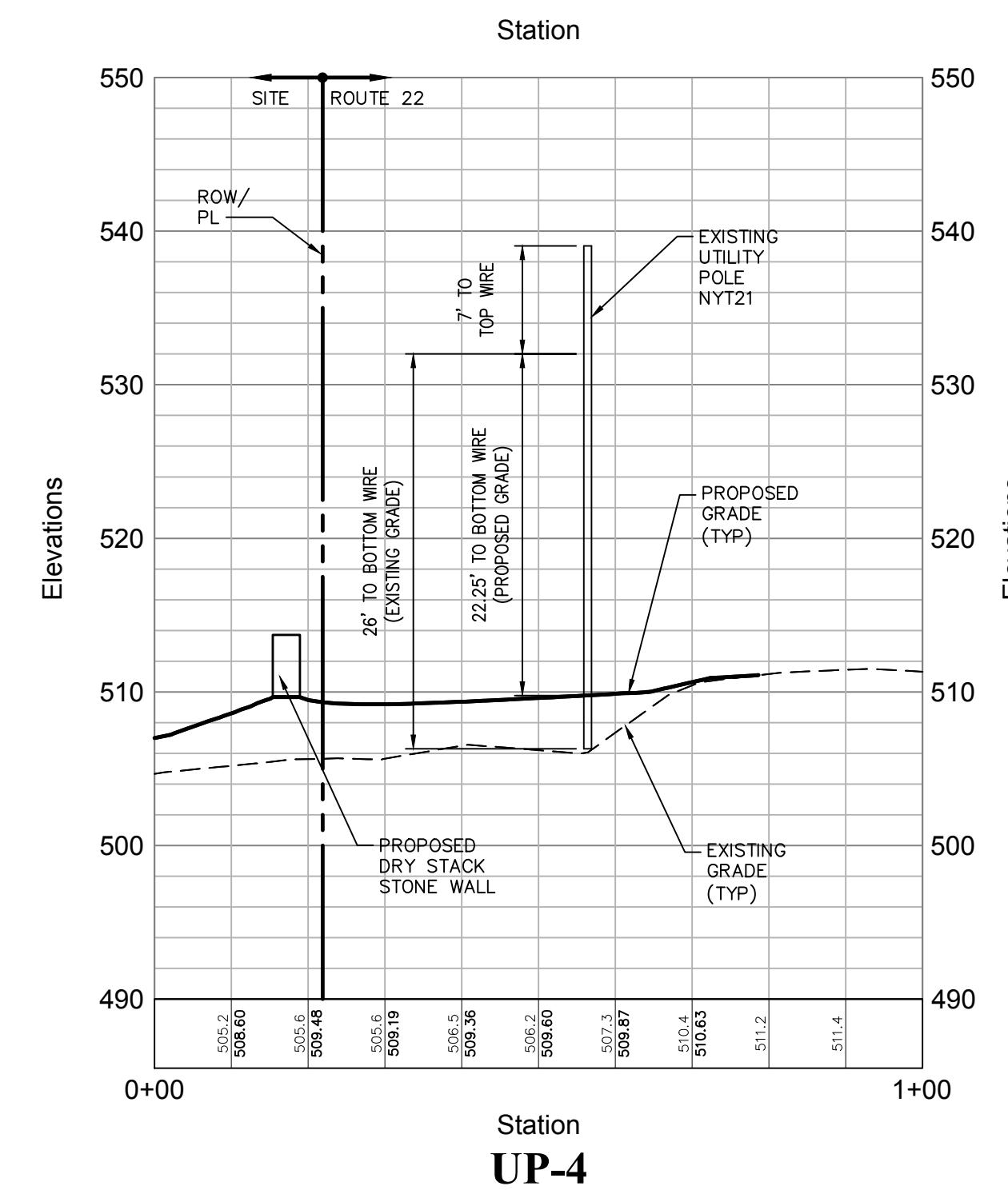
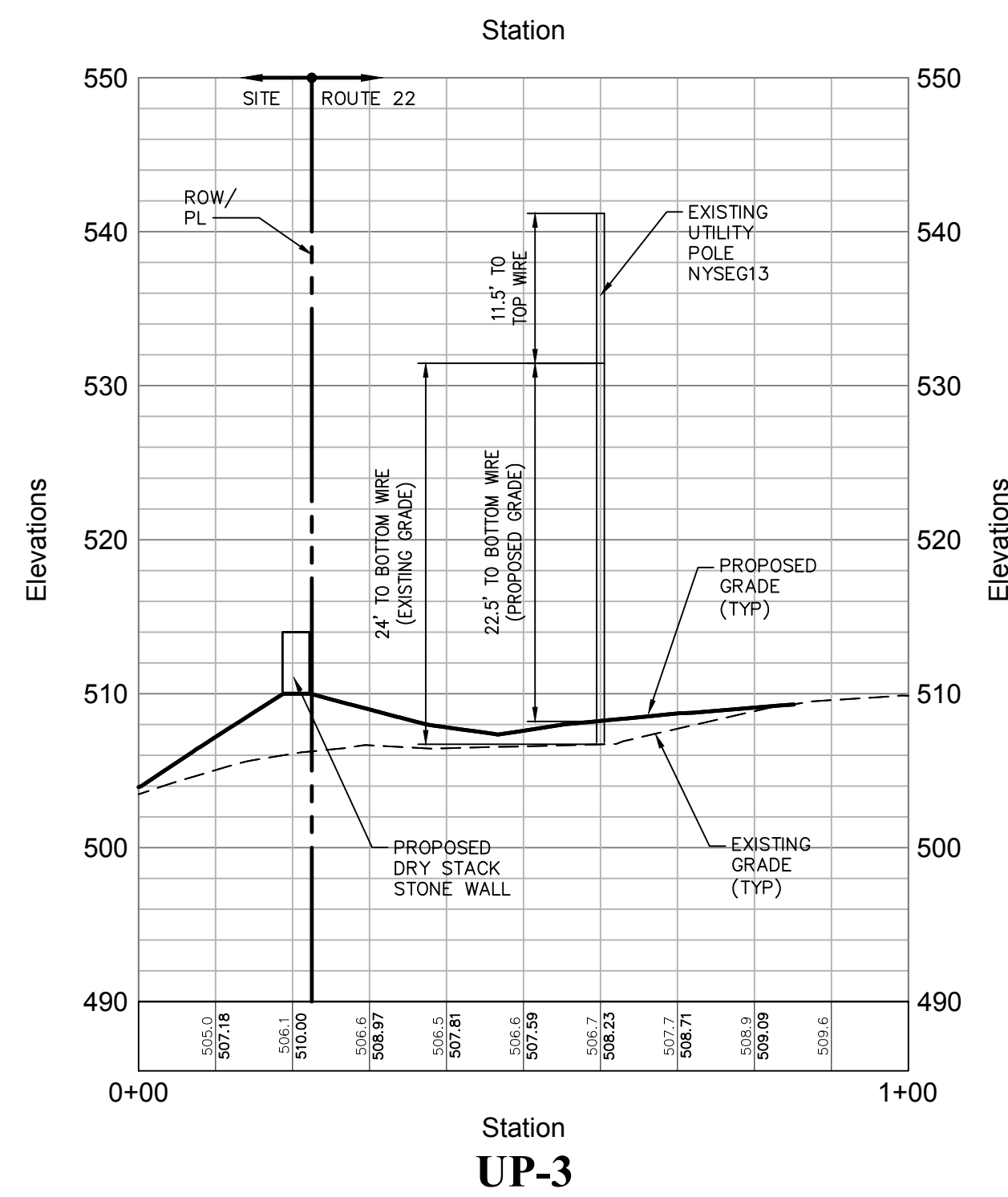
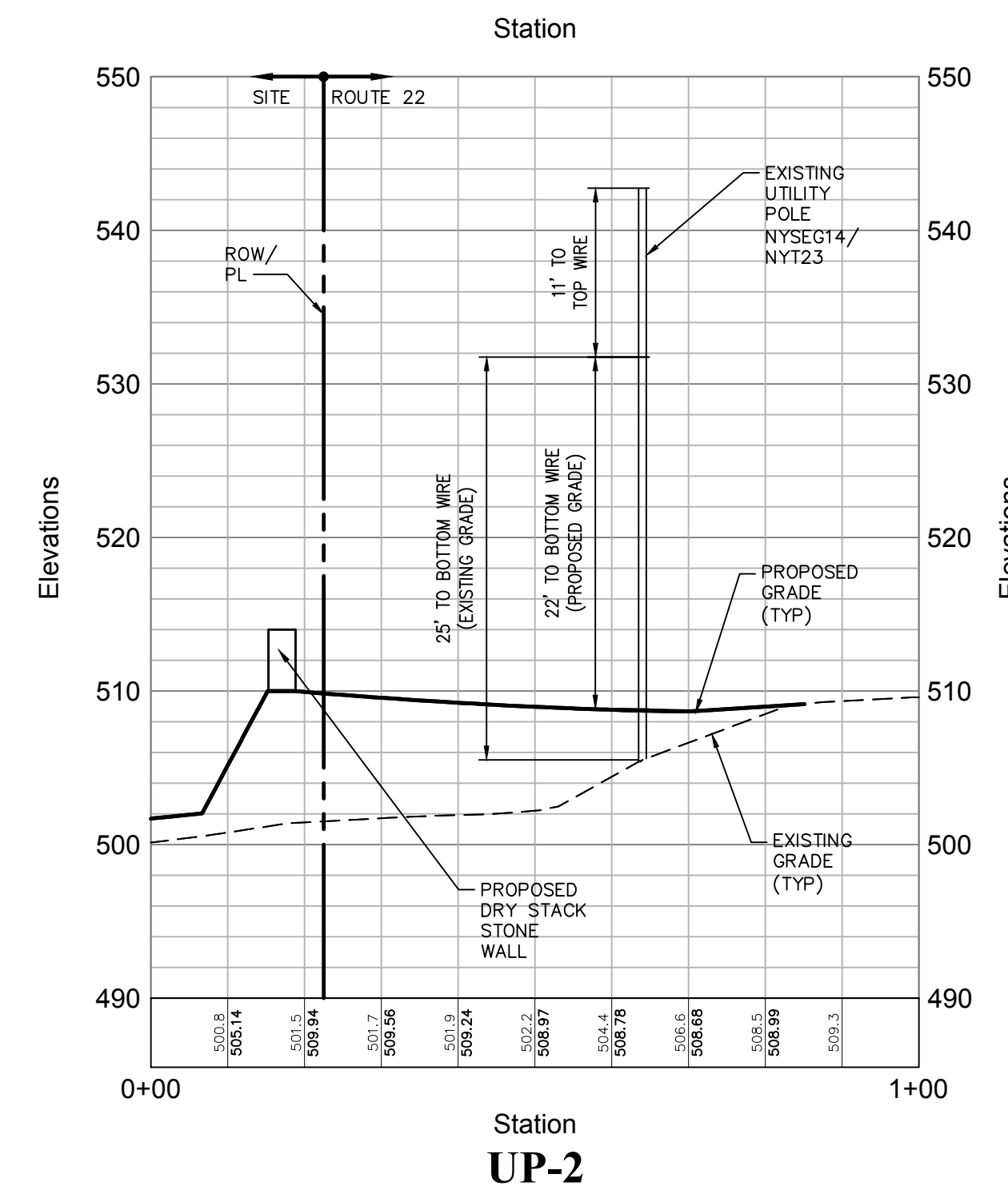
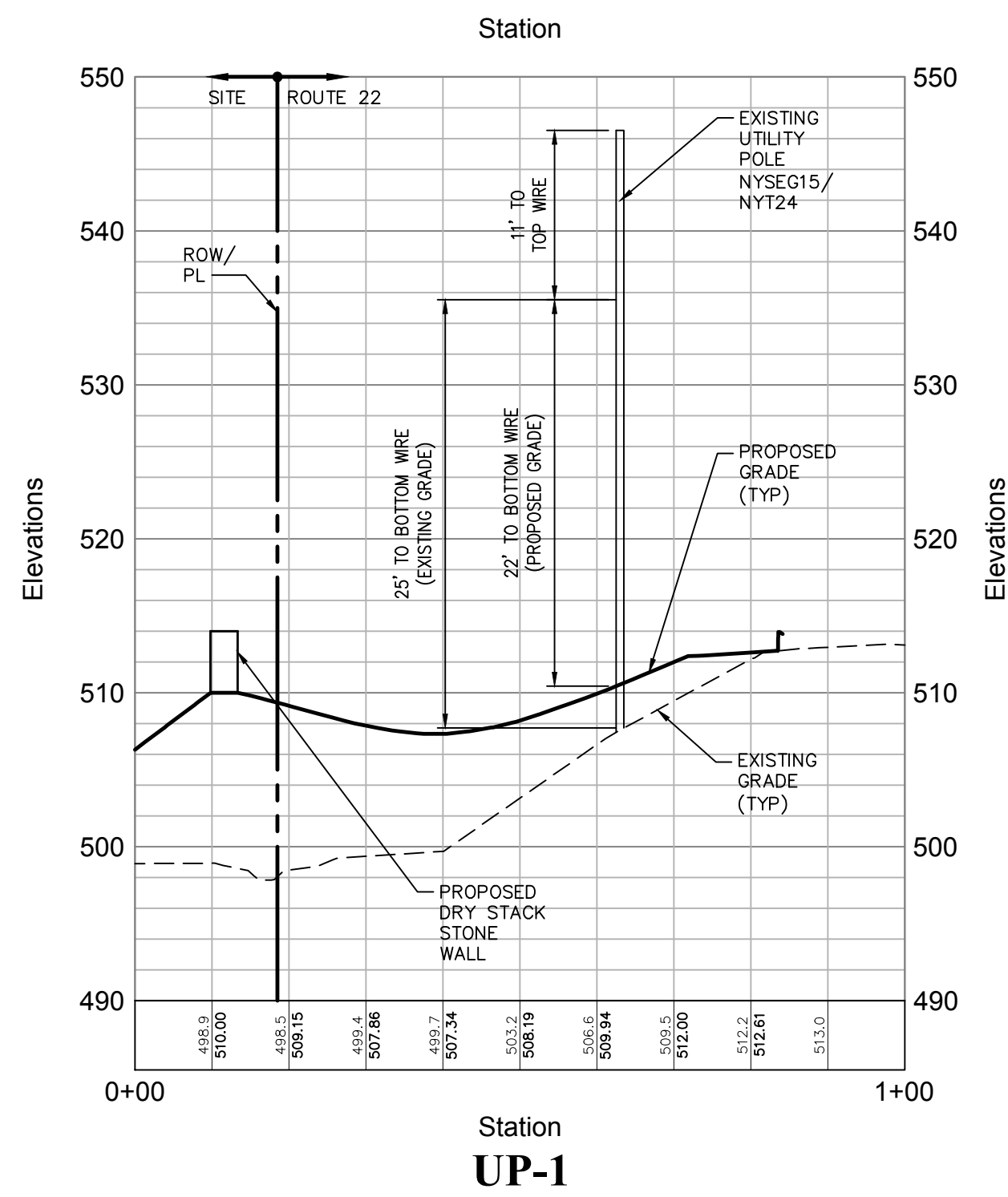
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Designed by	ACD	Drawn by	DAR	Checked by	MWJ
CAD checked by	MB	Approved by	ACD		
Scale	As Shown	Date	January 8, 2015		
Project Title					

**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York

Issued for
NYS DOT Permit

Construction
Drawing Title

Miscellaneous Details 3

FILL CROSS SECTIONS AT MAIN ENTRANCE

Notes:
1. SEE SHEET GP-1 FOR PLAN VIEW.

Drawing Number
MD-3
Sheet of 16 18
Project Number
29011.00

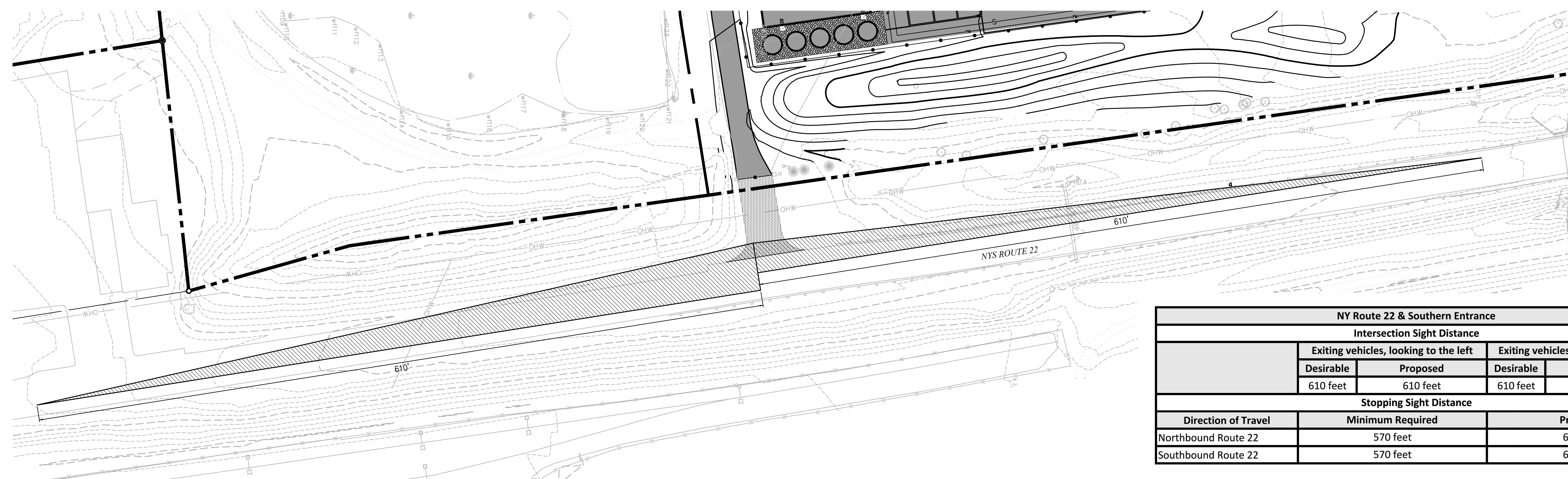
Amanda C. DeCesare
N.Y. Professional Engineer
NY Lic. No. 084690



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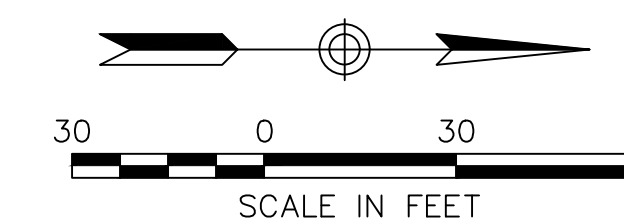
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NY Route 22 & Southern Entrance				
Intersection Sight Distance				
	Exiting vehicles, looking to the left		Exiting vehicles, looking to the right	
	Desirable	Proposed	Desirable	Proposed
	610 feet	610 feet	610 feet	610 feet
Stopping Sight Distance				
Direction of Travel	Minimum Required		Proposed	
Northbound Route 22	570 feet		610 feet	
Southbound Route 22	570 feet		610 feet	

SOUTH ENTRANCE - SIGHT DISTANCE TRIANGLES



NY Route 22 & Main Site Entrance				
Intersection Sight Distance				
	Exiting vehicles, looking to the left		Exiting vehicles, looking to the right	
	Desirable	Proposed	Desirable	Proposed
	610 feet	610 feet	610 feet	610 feet
Stopping Sight Distance				
Direction of Travel	Minimum Required		Proposed	
Northbound Route 22	570 feet		610 feet	
Southbound Route 22	570 feet		610 feet	

MAIN ENTRANCE - SIGHT DISTANCE TRIANGLES

No.	Revision	Date	Appvd.
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Scale	As Shown	Date	January 8, 2015
Project Title			

**Silo Ridge
Resort Community**

4651 Route 22
Amenia, New York
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NYS DOT Permit

Construction
Drawing Title

**Sight Distance
Triangles 1**

Drawing Number

SD-1

Sheet of
17 18

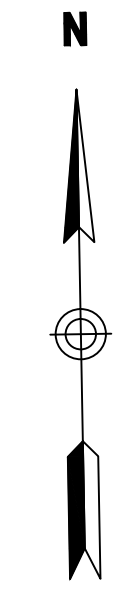
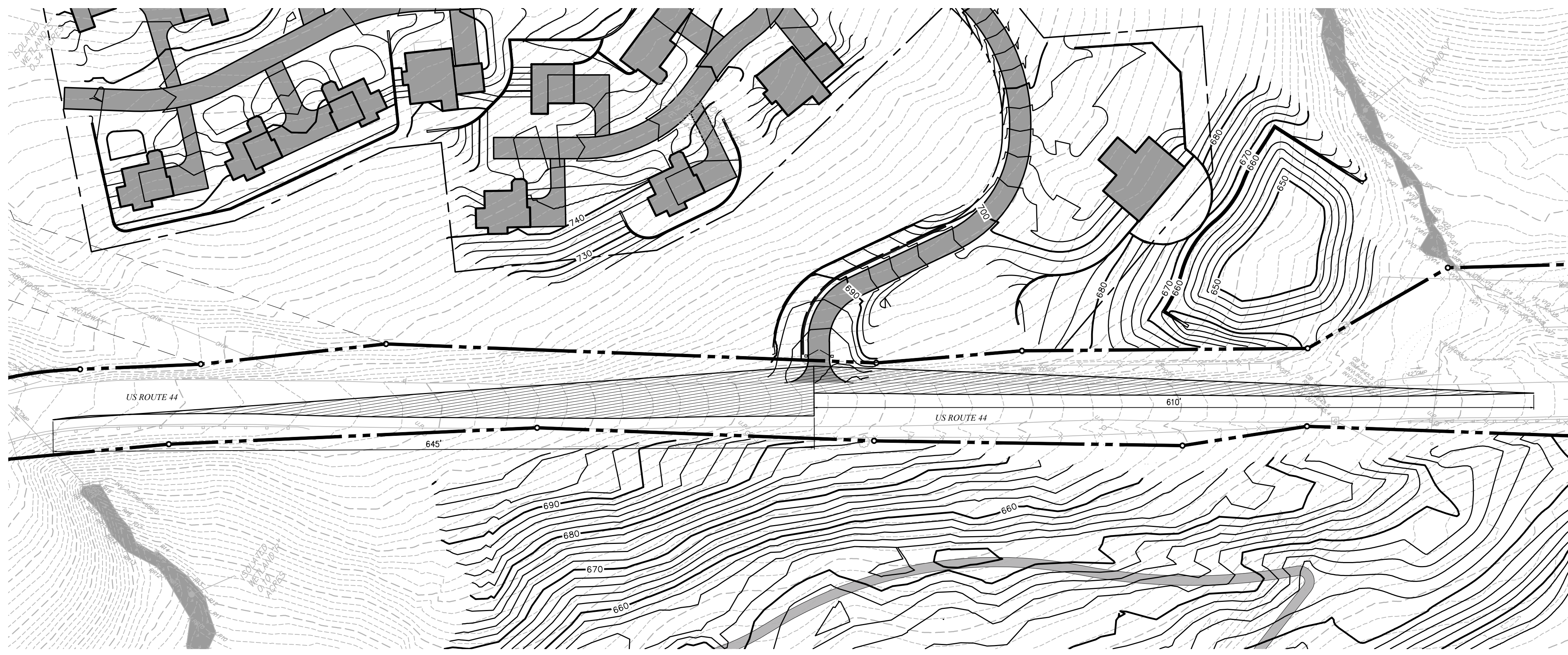
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NY Lic. No. 084690

Project Number
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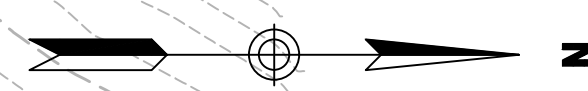
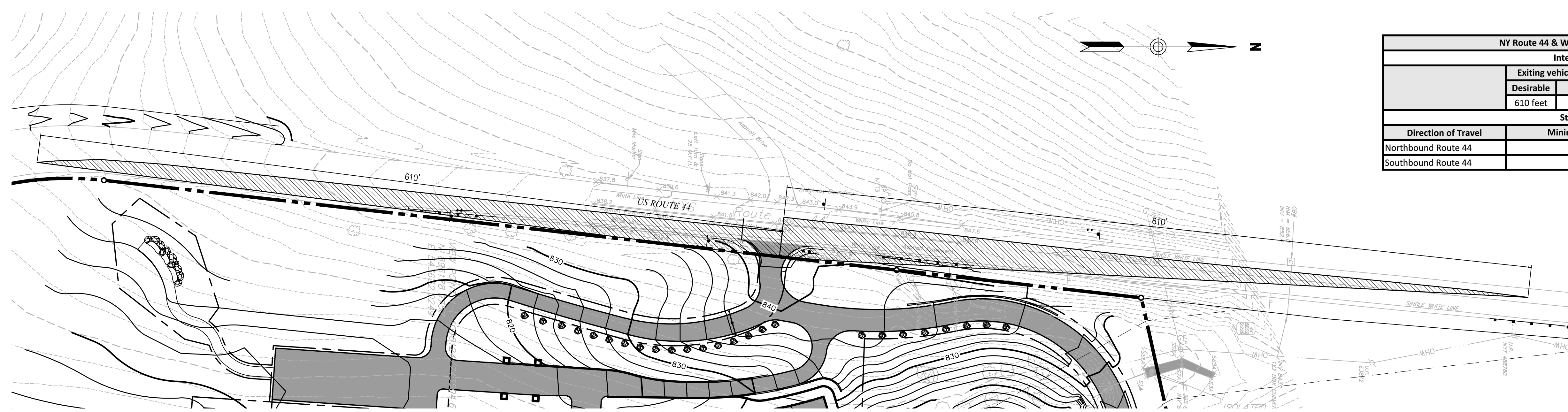


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NY Route 44 & Vineyard Estates Entrance				
Intersection Sight Distance				
	Exiting vehicles, looking to the left		Exiting vehicles, looking to the right	
	Desirable	Proposed	Desirable	Proposed
	610 feet	610 feet	610 feet	645 feet
Stopping Sight Distance				
Direction of Travel	Minimum Required		Proposed	
Eastbound Route 44	645 feet*		645 feet	
Westbound Route 44	500 feet*		610 feet	

VINEYARD ESTATES ENTRANCE - SIGHT DISTANCE TRIANGLES



NY Route 44 & Winery Restaurant/Overlook Entrance				
Intersection Sight Distance				
	Exiting vehicles, looking to the left		Exiting vehicles, looking to the right	
	Desirable	Proposed	Desirable	Proposed
	610 feet	610 feet	610 feet	610 feet
Stopping Sight Distance				
Direction of Travel	Minimum Required		Proposed	
Northbound Route 44	530 feet*		610 feet	
Southbound Route 44	600 feet*		610 feet	

WINERY ENTRANCE - SIGHT DISTANCE TRIANGLES

No.	Revision	Date	Appvd.

Designed by **ACD** Drawn by **DWB** Checked by **MWJ**
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 Project Title

**Silo Ridge
Resort Community**
 4651 Route 22
 Amenia, New York
 Issued for
 NYS DOT Permit

Construction
 Drawing Title
**Sight Distance
Triangles 2**

Drawing Number
SD-2
 Sheet of 18 18
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 N.Y. Professional Engineer
 NY Lic. No. 084690
 Project Number
 29011.00